



# FARLEIGH FIELDS, BACKWELL

Outline Application  
Design and Access Statement

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## Outline Application

## Design and Access Statement

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## Team

This Design and Access Statement has been produced and edited by RPS, with contributions and summaries from Charles Church and the following consultants who have provided additional detailed site analysis and mitigation requirements and helped prepare the planning application and accompanying documents.

- **RPS** - Masterplanning
- **Charles Church** - Planning
- **SLR** - Landscape
- **PFA** - Highways and Transportation
- **Callidus** - Flood Risk and Drainage
- **MWA** - Ecology
- **Cotswold Archeology** - Archeology
- **Consensus** - Consultation

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# SUMMARY

*This design and access statement is submitted in support of an outline planning application with all matters other than access from Farleigh Road reserved.*

*Detailed design and related matters will be dealt with through subsequent reserved matters applications, for which this design and access statement sets the framework and principles.*

*The proposal includes 7.29 ha of residential development which includes 1.73 ha of required open space. A further area of 10.32 ha is provided as additional open space.*



# VISION

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*The application site enjoys a highly sustainable location close to the heart of Backwell. The site is surrounded by existing development, and is within walking distance of the mainline train station, and is close to shops and schools.*

## *Description of Development:*

*Residential development of up to 220 new dwellings (Use Class C3) with supporting infrastructure and facilities including the demolition of 56 Farleigh Road, creation of a new vehicular access from Farleigh Road, open space, landscaping, drainage infrastructure and other associated enabling works.*

The development will reflect the scale and character of the village and will be based on established design principles. It will include the provision of market and affordable housing. The applicant's vision for the development of Farleigh Fields is:

- to provide a sustainable development in a sustainable settlement, well served by public transport and local amenities;
- to develop an attractive neighbourhood within Backwell;
- to provide a volume range and type of housing for which there is a requirement in North Somerset and particularly in Backwell;
- to focus development on about one-third of the site where there will be the least impact and the easiest access for vehicles and pedestrians;
- to make the remaining two-thirds available to the community as open space;
- to enhance spaces and places for amenity, play, wildlife and ecology to flourish; and
- to make a contribution to the quantity of new housing required in North Somerset.

The development will achieve this by:

- a considered process, represented in this document, to ensure the integration of the development within its surroundings;
- reasoned limits on scale and density;
- an appropriate approach to architecture, drawing on local context and character;
- the generous provision of open space and connecting footpaths; and
- a mix of housing types that will respond to a local need for affordable homes, starter homes and down-sizing homes.

# 1 INTRODUCTION

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*This design and access statement forms part of the outline planning application for residential development on land at Farleigh Fields.*

*It sets out the background to the proposed project and summarise the extensive design process, public consultation and site assessment and evaluation that has taken place in developing the proposal.*

## 1.1 Purpose of the Document

This design and access statement complies with the requirements of the Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2013, Sections 4(2) and 4(3), and is structured in accordance with *Design and access statements How to write, read and use them*. [CABE 2006].

The statement includes the following sections:

- **Section 1 Introduction** – information about the purpose and structure of the statement, and background and context to the application.
- **Section 2 Assessment** – describes contextual and physical site issues
- **Section 3 Involvement** – describes the process and findings of community involvement.
- **Section 4 Evaluation** - evaluates the information presented in Section 2 and illustrates the process of design evolution undertaken.
- **Section 5 Design Parameters** – describes design proposals that support the masterplan, and the parameters and strategies for the development.
- **Section 6 Design Strategies and Principles** – sets out the strategic implementation and delivery strategy for the development.

## 1.2 Site Location and Description

The site lies within Backwell and is enclosed by existing development. It is approximately 8 miles from Bristol and is well connected by public transport. Backwell train station is about 2km from the site and has mainline connections to Bristol, Exeter and beyond.

The site is irregular in shape and covers an area of 21.04 hectares.

The site is accessed from Farleigh Road. There is also pedestrian access from the southern boundary.



St Andrew's Church



Looking north from the site



Looking south from the site

 Application Site  
(indicative only)



# 2 ASSESSMENT

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*“The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.”*

*(para 56 of the NPPF)*



## 2.1 Planning Context

### NATIONAL PLANNING POLICY

The National Planning Policy Framework sets out the Government's policies for England. It establishes a presumption in favour of sustainable development in respect of economic, social and environmental considerations.

Good design is a key aspect of sustainable development and the framework sets out objectives relating to functionality and creation of a sense of place. A good design should consider the ability of a place to function well, establish a strong sense of place, optimise the potential of the site to accommodate development and create and sustain a mix of uses, respond to local character and history, reflect the identity of local surroundings and materials, create safe and accessible environments and visually attractive places.

### NATIONAL PLANNING GUIDANCE

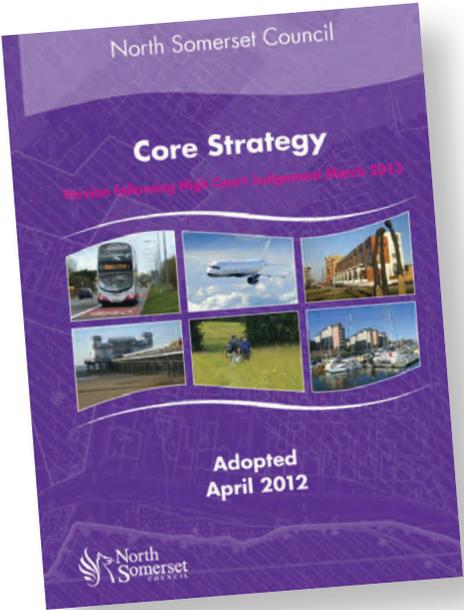
The National Planning Practice Guidance states that good quality design is an integral part of sustainable development and that achieving good design is all about creating places, buildings or spaces that work well for everyone, look good and adapt to the needs of future generations. Good design responds in a practical and creative way to the function and identity of a place.

Well designed developments should be functional, support mixed uses and tenures, produce successful public spaces, are adaptable and resilient, have a distinctive character, are attractive and encourage ease of movement.

The NPPG sets out more detailed guidance on character and landscape, local context, scale, detailing, quality of public realm, ease of movement and the quality of streets, adaptability, diversity of uses, access for disabled people, designing out crime and passive solar design.

**LOCAL DEVELOPMENT PLAN**

The Development Plan for Backwell comprises the saved policies within the North Somerset Local Plan together with the adopted North Somerset Core Strategy and Neighbourhood Plan. The site lies outside the existing settlement boundary for Backwell but is entirely surrounded by existing developments which is within the settlement boundary. A full assessment of the planning policy context is provided in the accompanying planning statement.



**North Somerset Core Strategy**

The North Somerset Core Strategy was adopted in April 2012. However, in March 2013 a judgement following a legal challenge to the document concluded that the Inspector who undertook the Examination failed to give adequate or intelligible reasons for his conclusion that the North Somerset housing requirement made sufficient allowance for latent demand. As a consequence Policy CS13 relating to the housing requirement was remitted back to the Planning Inspectorate for re-examination together with 8 other policies relating to green belt and distribution of development.

The examination of the remitted policies is currently taking place and the first hearings were held in March 2014 in respect of the overall housing requirement. An inspector’s letter was received by the council on 22nd April 2014 in which he says that the housing requirement should be increased. The council responded on 24th July 2014 proposing Main Modifications to increase the housing provision.

The Inspector invited the council to consult on these modifications before reopening the re-examination. This consultation concluded on 5th November 2014. The Inspector will consider the responses to the consultation, before deciding how to proceed with the re-examination.

Other than the remitted policies, all other policies in the plan remain adopted. Of particular relevance Policy CS2 sets out criteria against which development proposals will be assessed. These include:

1. A requirement for designs that are energy efficient and designed to reduce their energy demands;
2. The use of on site renewable energy sources or local off-site renewable energy sources to meet a minimum of 15% or 10 or more dwellings;
3. A minimum code for sustainable homes level 3 for all new dwellings from October 2010, level 4 from 2013 rising to level 6 by 2016;
4. Developments of 10 or more dwellings to incorporate 50% constructed to life time home standard up to 2013 and 100% from 2013 onwards;
5. Require the application of best practice in sustainable drainage systems to reduce the impact of additional surplus water run-off from new development.

In addition, Policy CS12 sets out standard for achieving high quality design and place making.

## Replacement Local Plan (2007)

The relevant design policy in the replacement local plan which remains extant is Policy GDP/3. This says that poorly designed development proposals will be rejected and proposals will be determined against a range of criteria, including whether:

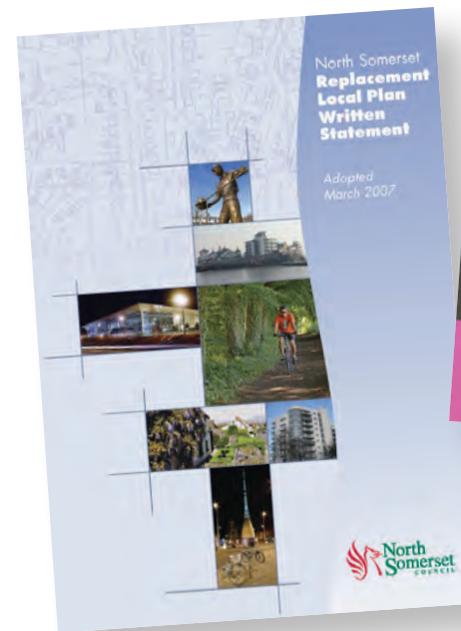
1. The proposal safeguards and integrates important features such as areas of ecological, amenity or heritage value, views of public importance across the site and rights of way;
2. The siting, levels, density, form, scale, height, massing, detailing, colour and materials respect the characteristics of the site and its surroundings and avoids adverse effects within the site and upon adjoining areas through overlooking or overshadowing or overbearing impact;
3. The external areas form an integral part of the design, provide permeability and are of a scale and character appropriate to the development proposed and the surrounding area;
4. The hard and soft landscaping forms an integral part of the design and incorporates important existing features;
5. The proposal includes elements to reduce opportunities for crime through design and ensuring clear distinctions between private and public spaces;

6. The proposal enables private and emergency vehicles vehicular access to the site.
7. The proposal makes provision for convenient, safe and attractive access for pedestrians, cyclists, public transport and linkages to wider networks.
8. Whether the proposal makes a positive contribution to a high level of energy saving.

This policy will be replaced by policies in the Emerging Sites and Policies DPD but in view of the legal challenge to the Core Strategy this document has very limited weight at present.

## Parking Standards SPD

North Somerset's parking standards for new developments are set out in a Parking Standards SPD adopted in November 2013. It provides minimum parking standards required for all new classes of development.



### Backwell Neighbourhood Plan

The Backwell Neighbourhood Plan was ‘made’ by the North Somerset Council on the 24th March 2015 as a result of the Backwell Plan referendum which voted in favour of the Plan. Charles Church have responded at each stage of the process, identifying the suitability of Farleigh Fields for development and objecting to the proposed designation of the land as Local Green Space.

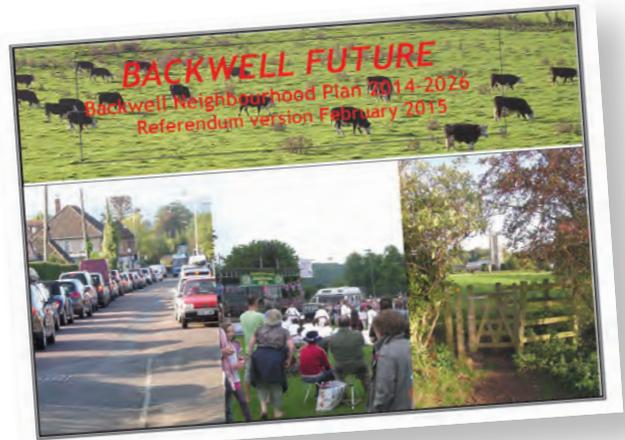
The Examiner’s Report was published in October 2014. To proceed to a Referendum, the plan must meet the Basic Conditions set out in paragraph 8 (2) of Schedule 48 to the Town and Country Planning Act 1990 and Section 38A of the Planning and Compulsory Purchase Act 2009. The Examiner has recommended

that the Backwell Neighbourhood Plan should proceed to Referendum, subject to a substantial number of modifications, including those directly relevant to this planning application set out as follows.

The Examiner recommended deleting the entire section on Local Green Space, the policy and the Local Green Space Map. He found that Farleigh Fields comprises ‘an extensive tract of land,’ that there was ‘no substantive or compelling evidence’ to show that it is not and that the Local Green Space designation has ‘direct conflict with national policy’ and ‘does not meet the basic conditions’.

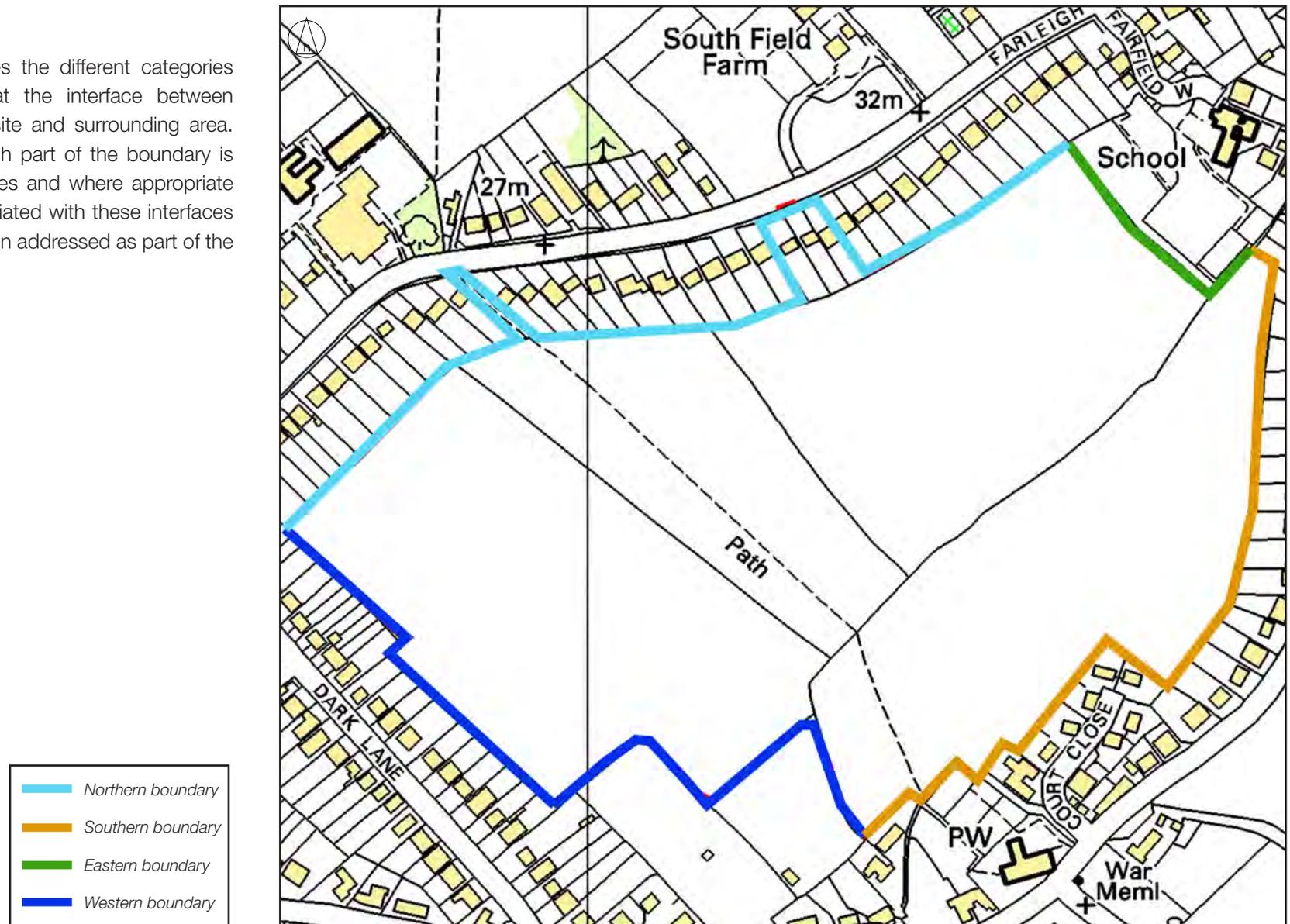
The Examiner further concludes that his recommendation does not mean that the areas for which Local Green Space designation was sought will automatically become available for development and that consideration will be based on national and local planning policy. The DAS and the accompanying Planning Statement sets out why the development at Farleigh Fields proposed in this planning application is in accordance with national and local planning policies.

The Neighbourhood Plan contained no specific design policies, but policies Sustainability 2 and 3 and Development Policy 6 contained requirements which would have had design implications. However, the Inspector has now recommended that these should be deleted.



## 2.2 Site Boundaries

The plan opposite identifies the different categories of boundary treatments at the interface between the proposed application site and surrounding area. A broad description of each part of the boundary is given on the following pages and where appropriate design consideration associated with these interfaces are described and have been addressed as part of the evaluation in Section 4.



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**Northern Boundary**

To the north the site is bounded by residential properties, predominantly large detached houses with typically very deep rear gardens of approximately 40m. The boundary is formed by close-boarded fencing and some hedges. An existing public right of way connects through this boundary onto Farleigh Road.



*Northern boundary*



*Southern boundary*

**Eastern Boundary**

To the east the site is bounded by the playing fields for Fairfield School and residential properties. The eastern boundary is formed by fencing and some hedges.

**Southern Boundary**

To the south the site is bounded by residential properties and Backwell Church Town Conservation Area. The site boundary is formed by fencing with some mature trees and hedges.



*Western boundary*



*Eastern boundary*

**Western Boundary**

To the west the site is bounded by residential properties. Fencing with some tree planting extends along the boundary.

## 2.3 Local Facilities and Services

### Retail / Commercial Facilities

There is a convenience store, post office, restaurant, hardware store, estate agents, doctors' surgery and pharmacy located close to the junction of Farleigh Road and West Town Road.

Further shops and facilities, including a hairdressers', veterinary surgery and off-licence, are located on Rodney Road, a short walk from the site.

Although the number of village shops have declined in recent years, in total there are 24 shops and a Post Office in the village. Additional facilities include Backwell Medical Centre, Backwell Dental Centre, three pubs, a village club, two restaurants, two takeaways and play groups.

In October 2014 North Somerset Council recommended approval of a planning application for the erection of 15 industrial units for class B1A, B1B, B1C and B8, subject to a Section 106 Agreement. This will provide new employment opportunities within walking distance of Farleigh Fields.

There are additional shops and facilities, including Waitrose and Tesco, located in Nailsea, which is approximately 2 miles from the site, accessible by public transport.

### Education

The nearest school is Backwell C of E Junior School located to the south, approximately 500m from the site. The nearest infants' school, West Leigh, is located approximately 1.5km from the site. Fairfield School is immediately to the east of the site, and Backwell School to the north of Farleigh Road with its main access on Station Road.

### Open Space

This is considered in Section 2.7 of this document.



*Backwell Leisure Centre*

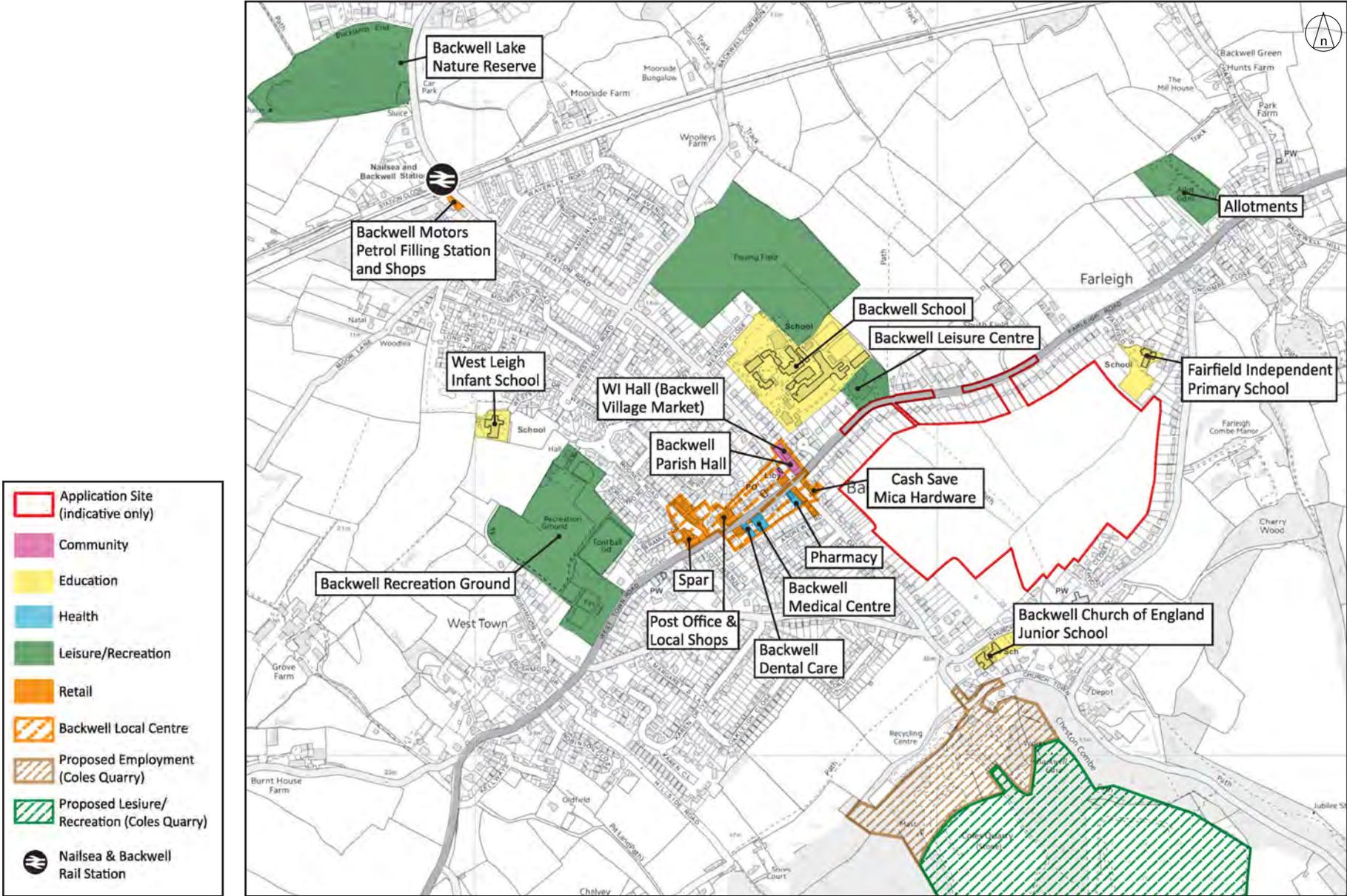


*Backwell Community Hall*



*Backwell local centre*

### Facilities Plan



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## 2.4 Transport and Movement

The application site is located to the south of the A370 Farleigh Road. The A370 runs between Bristol and Weston-super-Mare. In the vicinity of the application site, Farleigh Road currently carries approximately 14,000 vehicles a day on weekdays. It is a single carriageway road with footways on both sides, it is street lit and is subject to speed limits of 30mph to the west and 40mph to the east of the existing site access, which is a track located between numbers 30 and 32 Farleigh Road. To the west of the application site, Farleigh Road connects with Station Road and Dark Lane at Backwell crossroads. Station Road provides a route to Nailsea & Backwell Rail Station and towards Nailsea beyond the Station.

### Pedestrians and Cyclists

Two public footpaths cross the application site: (public footpaths LA2/6/10 and LA2/4/20) providing routes between Farleigh Road and Church Lane.

Footways are present on both sides of Farleigh Road in the vicinity of the application site and Backwell crossroads is equipped with a full pedestrian stage (ie all vehicular approaches are stopped whilst pedestrians are signalled across all junction arms).

Cyclists are required to cycle in the carriageway in Backwell and advanced cycle stop lines are present at Backwell Cross Roads. Cycle parking is provided at the Rodney Road shops in Backwell and at Nailsea & Backwell Rail Station.

### Public Transport Links

Backwell is well served by bus services, with 4 to 6 services per hour to and from Bristol at peak times. There are also frequent and direct bus services towards Weston-super-Mare, Nailsea and Clevedon and a commuter service to Bristol International Airport.

In addition, Nailsea & Backwell Rail Station is served by frequent regional trains towards Bristol, Cardiff, Weston-super-Mare and Taunton and peak time direct services to and from London Paddington and Exeter St Davids.

### Parking Strategy

Car and cycle parking at the proposed development would be provided in accordance with NSC's supplementary planning document, North Somerset Parking Standards.



*A bus stop on Farleigh Road*

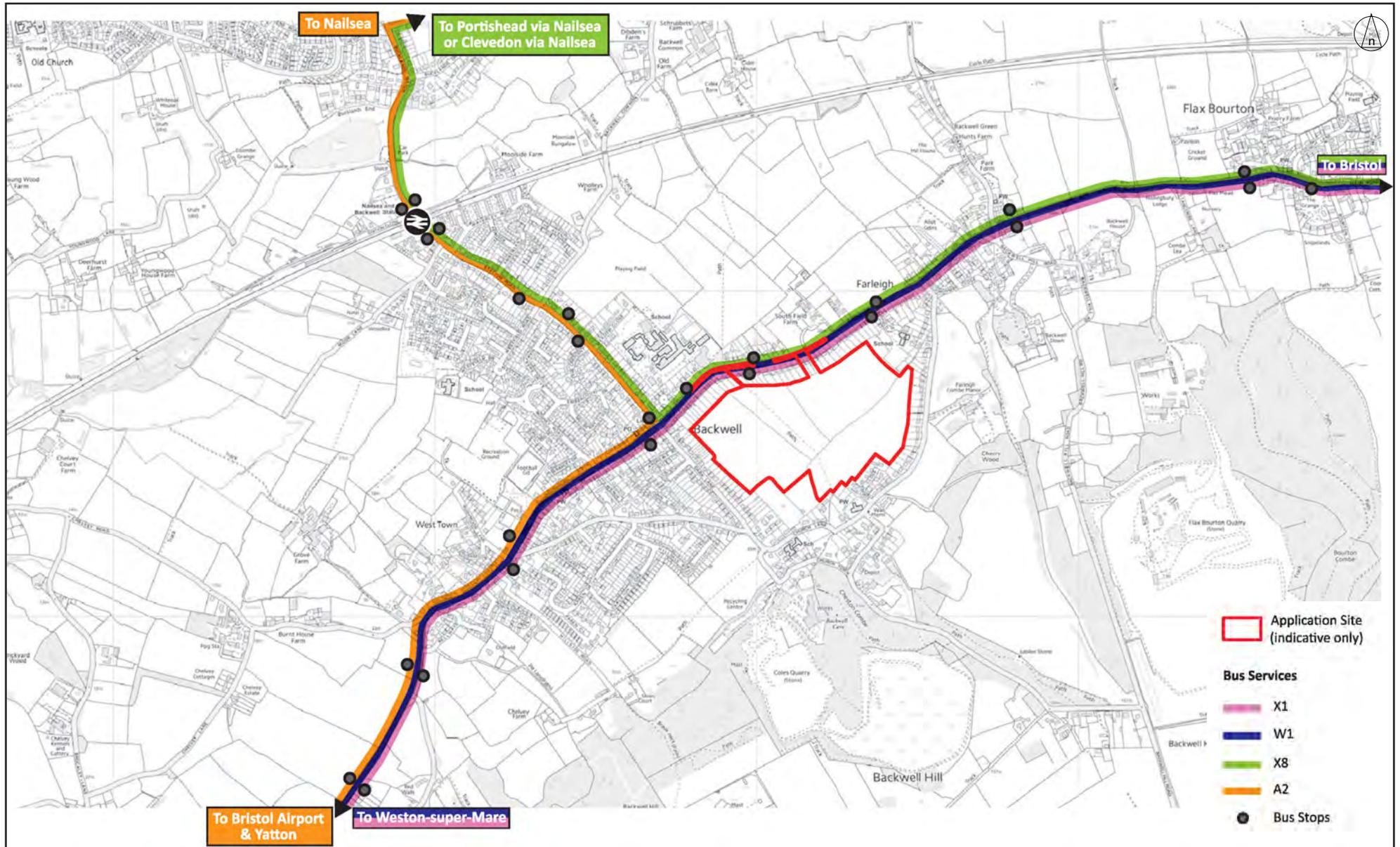


*Backwell train station*



*The existing public right of way access to site*

### Local Bus Routes Plan



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## 2.5 Heritage Context

In July 2014 a Heritage Desk-Based Assessment was conducted by Cotswold Archaeology, which assessed the archaeological potential of the site and possible development impacts upon heritage assets. Following consultation with Vince Russet, North Somerset County Archaeologist, a geophysical survey was carried out by Pre-Construct Geophysics on areas of proposed development in the north of the site. A summary of the findings of the assessment and survey work are provided below.

No designated heritage assets are recorded within the site. A single non-designated heritage asset is recorded within the site, namely a large stone slab which has been recorded c.200m north of the Church of St Andrew. This feature has been attributed a Neolithic date, although no details are provided within

the HER to support this, and the asset itself could not be verified during the site visit. Analysis of aerial photography suggests that the slab may relate to a structure formerly located within the site.

An assessment of the known archaeological resource of the site and its environs, together with the general topography of the area, would suggest that any settlement activity related to the prehistoric and Roman periods would be likely to be focused on the higher ground of Backwell Hill, or the ridge on which Church Town is now located.

The site is considered to have been situated beyond the focus of the medieval hamlet of Church Town, and there is therefore limited potential for remains of medieval settlement within the site, particularly

within the areas of proposed development. The site would appear to have remained within the agricultural hinterland of Church Town, and the growing Backwell settlement, from the medieval period until the present. On this basis any unknown archaeological features dating to the medieval and post-medieval periods are likely to comprise agricultural features of low archaeological significance.

Preliminary results of the geophysical survey would appear to indicate the location of a paleo-channel within the eastern area of proposed development, alongside a number of possible archaeological features, including potential ditches. While there exists a recognised correlation between the location of paleo-channels and recorded archaeological settlement remains,



preliminary results for this site indicate a higher density of possible geophysical anomalies within the western half of the proposed development area. A more detailed discussion on the results of the geophysical survey will be presented in a forthcoming report. It is possible that the presence of potential anomalies may result in the requirement for trial-trenching within the site, as previously stated by Mr Russet. Such evaluation would aim to target those anomalies identified by the geophysical survey to assess whether the feature is of archaeological origin.

Internal field boundaries within the site are considered to be remnants of early medieval enclosure. The design proposals have retained these boundaries, with the only identified physical impact being the construction of two new openings. Such impacts are considered to be minimal in extent, and will not harm the overall historic landscape character of the site ie. as medieval enclosed fields, particularly due to the high level of open space retained. Existing public rights of way which cross the site are considered to be reflective of historic route ways between the three hamlets, and to contribute to the overall character of the site as a reflection of the historical development of the area. These routes will be retained by the proposed development.

An assessment of potential impacts upon the setting of designated heritage assets within 1km of the study area identified the setting of only one heritage asset as being potentially sensitive to proposed development

within the site: Backwell Church Town Conservation Area. The Backwell Church Town Conservation Area contains one Scheduled Monument, one Grade I Listed Building, and nine Grade II Listed Buildings. Based upon an assessment of setting impact conducted in accordance with English Heritage guidance, the proposed development will not result in the alteration of the important elements of the setting of any designated heritage assets within the Conservation Area or the Conservation Area itself. The significance of these assets will therefore remain unharmed.

## 2.6 Ecology

A preliminary ecological survey of the application site and its surroundings was undertaken in 2013 by Michael Woods Associates.

The survey area was found to comprise four agricultural fields that were bordered by a network of species-poor intact and defunct hedgerows and tree lines. The most ecologically valuable and important habitats on the site were the uncultivated field margins and hedgerows. Although not particularly diverse or species-rich, such linear features nonetheless provide important habitat for British wildlife, in particular bats, reptiles and birds. Further detailed studies of these species groups were therefore undertaken in 2014.

The site lies within 4km of the North Somerset and Mendip Bats Special Area of Conservation; Brockley Hall Stables Site of Special Scientific Interest forms part of the SAC and is situated approximately 2.4km south-west of the closest part of this composite site. In line with best practice guidelines and local planning policy, a series of bat surveys were carried out in order to assess the potential impacts of the development upon bats, especially greater horseshoe and lesser horseshoe bats, for which the nearby SAC has been designated.

Overall the site was considered to support a moderate level of bat activity and high species diversity with a total of eleven species recorded during the surveys. The hedgerows on site were found to be of most

importance to foraging and commuting bats with low levels of bat activity observed in the centre of the fields. Common pipistrelle was the most abundant species on site with soprano pipistrelle and serotine bats also frequently recorded. Greater and lesser horseshoe bats were recorded on site in low numbers though the level of use did not indicate that the site was of particular importance for these bats. The risk of significant impacts upon the SAC populations were consequently dismissed.

A low population of slow worms was observed within the field margin habitats across the site with no reptiles being recorded within the field centres.

In total 32 bird species were recorded during the survey visits of which 15 species were considered to be definitely or probably breeding on site. Most of the bird assemblage could be considered to be typical of arable and pasture farmland within a network of hedgerows on urban fringe habitats.

A Main badger sett was recorded within the south-west boundary of the site and an Outlier sett and Annex sett were recorded within one of the internal hedgerows.

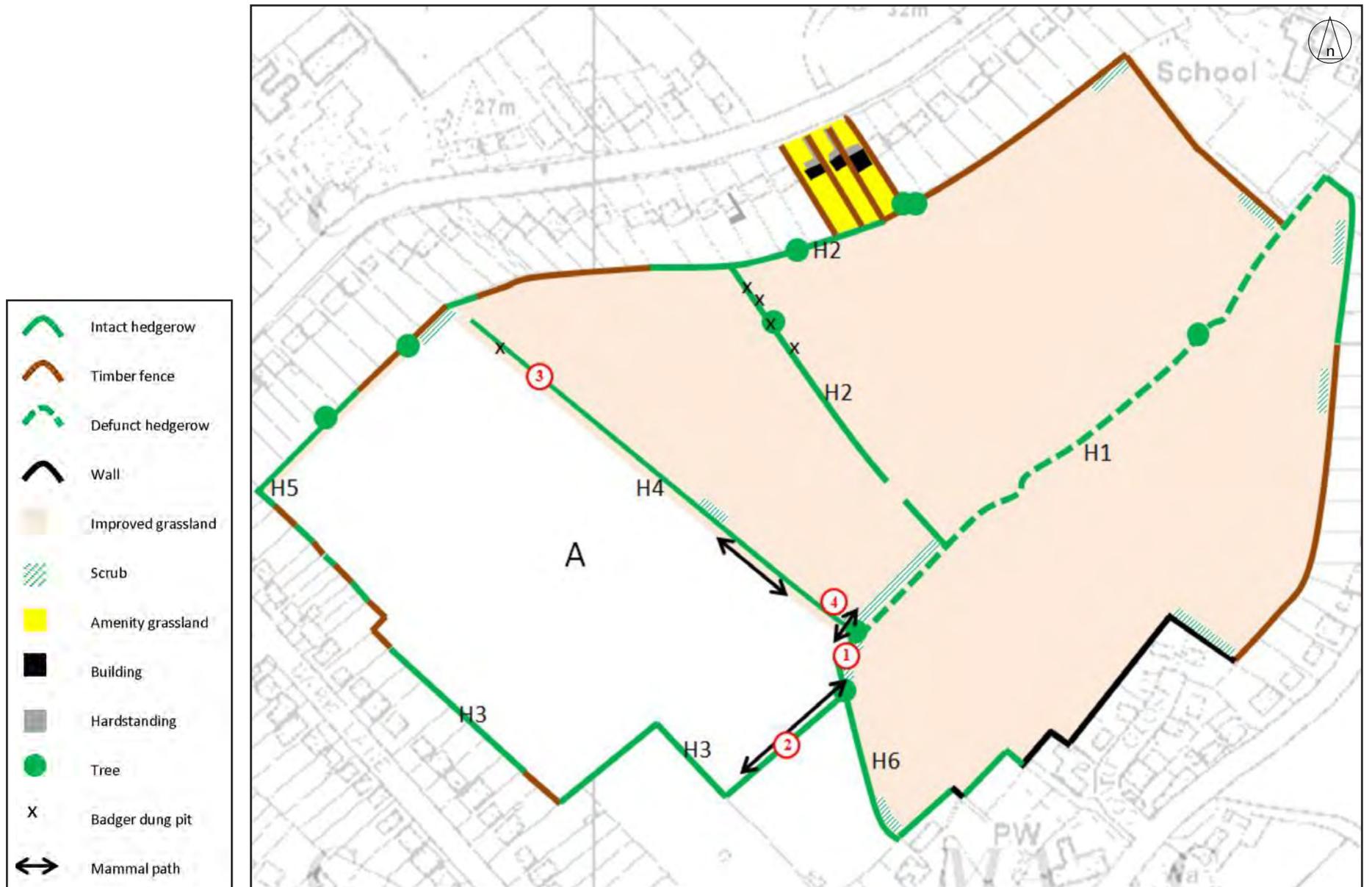
Detailed mitigation measures have been proposed to minimise the adverse impacts of the development and ensure the favourable conservation status of the species recorded on site, these include:

- Retention of the majority of the agricultural land as majority of the site to be undeveloped and available for community use

- Retention of all internal hedgerows on site to maintain wildlife corridors and facilitate dispersal.
- Extensive tree planting along the field boundaries to enhance the important hedgerow habitats and reduce the adverse impacts of increased artificial lighting
- Creation of an area of species-rich grassland to enhance the site for wildlife
- Creation of a water body which will enhance the site for a range of wildlife
- Production of a detailed lighting strategy to minimise the impacts of increased artificial light
- Production of a Reptile Mitigation Strategy to ensure no reptiles are injured during construction
- Production of a Landscape and Ecological Management Plan which will set out how all ecological receptors will be protected during construction and how retained habitats will be sensitively managed post construction.

The submitted ecology reports has concluded that the residual impact of the proposed development, assuming the successful implementation of the mitigation measures recommended within the report will not be significant on the general assemblage of bats.

Ecological Survey



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## 2.7 Open Space

The application site is privately owned land. The land is occupied by a farming tenant. Public access is limited to public footpaths. North Somerset Council has prepared draft Green Infrastructure Guidance which highlights an under-supply in Neighbourhood Open Space, Woodland and Conservation Sites within Backwell Parish. Overall, there is 4.3 hectares of open space provision within the Parish compared with North Somerset Council's requirement of 17.06 hectares. The application proposals would deliver up to 220 new homes which, based on the average household size for North Somerset of 2.3 people, would generate a new population of up to 506 and a need for up to 1.23 hectares of further required open space to meet the needs of the new residents (Local Plan 2007 para 1.11). Therefore, on completion of the development there would be a total need for 18.29 hectares of open space within the Parish.

In total, the masterplan proposals would secure the provision of 12.88 hectares of open space for Backwell Parish, including the field north of St Andrew's Church and the field which runs from Church Field to Farleigh Road both of which received the highest nominations from local residents for their beauty, aesthetic importance and recreational use (reference Backwell draft Neighbourhood Plan, page 39). The delivery of 12.88 hectares of open space would meet the needs of new residents at Farleigh Fields and would dramatically reduce the existing deficit of open space within the Parish from 12.76 hectares to approximately 7 hectares.

The development proposals would serve to open up Farleigh Fields to the local community as fully accessible open space with improved footpath links, new planting and a children's play area. This would significantly enhance the provision and access to neighbourhood open space south of the A370 in accordance with recommendations of the Council's draft Green Infrastructure Guidance. Charles Church has held discussions with St. Andrew's Church regarding the potential use of some of the land to extend the existing cemetery.

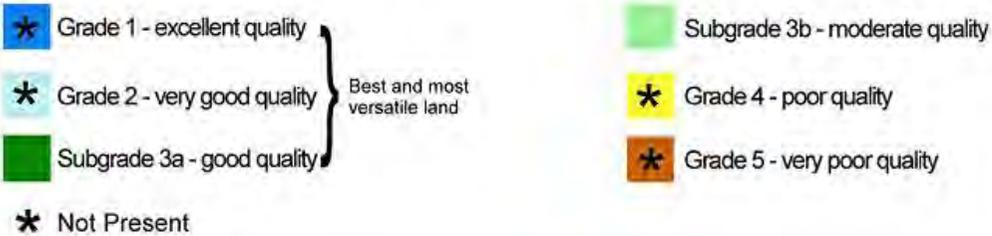
The draft neighbourhood plan had proposed the designation of Farleigh Fields as Local Green Space. However, the Examiner's Report in to the draft Neighbourhood Plan concluded that Farleigh Fields did not qualify against the criteria for Local Green Space specified by the NPPF and accordingly the proposed designation has been deleted. However, approval of this application would meet and exceed the aspirations of the draft Neighbourhood Plan, safeguarding those areas of land deemed most valuable to the local community and opening up the majority of the site for full public access in perpetuity. Charles Church is exploring various opportunities for the future management of the open space areas. Such arrangements will be formalised through a S106 legal agreement.

### 2.8 Agricultural Land Quality

The site has been owned by Charles Church and its predecessors for the last thirty years, during which time it has been farmed and let out on successive agreements to two local farmers, each occupying the land for around 15 years. The current farmer occupies the land on a short-term Farm Business Tenancy, and uses two of the fields for arable crops with the remainder in permanent pasture for grazing beef cattle. There are no farm buildings on the site, with the main farm centre and block of land located to the north of the site, such that agricultural access to the site is achieved mainly by crossing Farleigh Road.

As the site has not formed an integral part of an agricultural holding for many years, and it has been farmed on the basis and in the knowledge that it may not be available at the end of each agreement, the proposed development of the site would not affect the continued management of the residual main holding which will continue to be able to operate using its own buildings and land to the north of the site.

The proposed development will involve the loss of approximately 7.7ha of predominantly Subgrade 3a with a small area of Subgrade 3b land. The remaining land will remain undisturbed as open space, enabling the in-situ soils to continue to fulfil their various functions in storing water and carbon, and supporting biodiversity and landscape.



## 2.9 Landscape Setting and Assessment

### Site Landscape Context

The site lies to the south of the A370 Farleigh Road, and is encircled by existing residential development within Backwell and Church Town. The part of Church Town lying to the south of the site is designated a Conservation Area, and contains several listed buildings including the Grade II Church of St. Andrew. Fairfield School abuts the north-east corner of the site.

Tyntesfield, a Grade II\* Registered Park and Garden of Special Historic Interest lies some 2.2km to the north-east of the site. Barrow Court, a second historic parkland, lies some 2km to the east on the opposite side of a wooded landform.

The Green Belt boundary wraps around the eastern part of Backwell, and includes the Tyntesfield Estate.

### Landscape Character

The site and wider study area lie within Natural England's National Character Area 118: Bristol, Avon Valleys and Ridges.

The north-western two thirds of the site lie within North Somerset Landscape Character Type (LCT) J: Rolling Valley Farmland and associated LCA J5: Land Yeo and Kenn Rolling Valley Farmland. The remainder of the site (above the 50m AOD contour), together with Church Town, lies within LCT E: Limestone Ridges and Combes and associated LCA E6: Cleeve Ridges and Combes.

### Topography

Backwell and Church Town lie on the north-west facing side of the Land Yeo valley. The built footprint of Backwell lies between 10m and 50m AOD, and that of Church Town between 50m and 90m AOD. The site rises southwards from approximately 30m to 70m, with a small spur formation extending into its central, upper part.

Above Church Town, the valley side rises to over 160m AOD, and is characterised by woodland blocks and quarries..

The south-west facing valley side rises to over 150m AOD above Tyntesfield.

### Typical Views

Twelve viewpoint locations have been selected to represent the range of publicly available views towards the site, with reference to the specialist computer modelling and site assessment work. These viewpoints have been agreed with the local authority.

Local views into the site include those from the surrounding housing edge and adjacent public footpaths.

Medium and longer distance views (more than 0.5km distance) are available across the valley from higher lying land. In these views, the lower northern parts of the site are largely screened by the adjoining housing and associated vegetation adjoining the A370. The

higher lying southern margin of the site is more visible, but is seen lying below existing housing development along Church lane and Dark Lane. The tower of the Church of St. Andrew forms a local landmark in these views, with the wooded upper part of the south-west valley side forming a dominant skyline above and behind it.

The site is not visible from most of the Tyntesfield Estate, which is heavily planted. The selected viewpoint represents the available view from a short section of track running along the north-west edge of the formal parkland.

The rising, wooded landform of the south-west valley side screens the site in views from south of Church Town.

### Character and Landscape Sensitivity of the Site

The site comprises four agricultural fields of average condition, which are sub-divided by three internal hedgerows. The tree condition, and ecological, surveys have classified the hedgerows as being largely of average quality, and contain few significant trees. The central hedgerow, which runs east to west across the site, is particularly gappy and poor in quality, but has the potential to be brought back into good condition to form a more important landscape feature.

Two public footpaths crossing the site from east to west and north to south.

### Landscape Context Plan

The site is not covered by any landscape related planning designations.

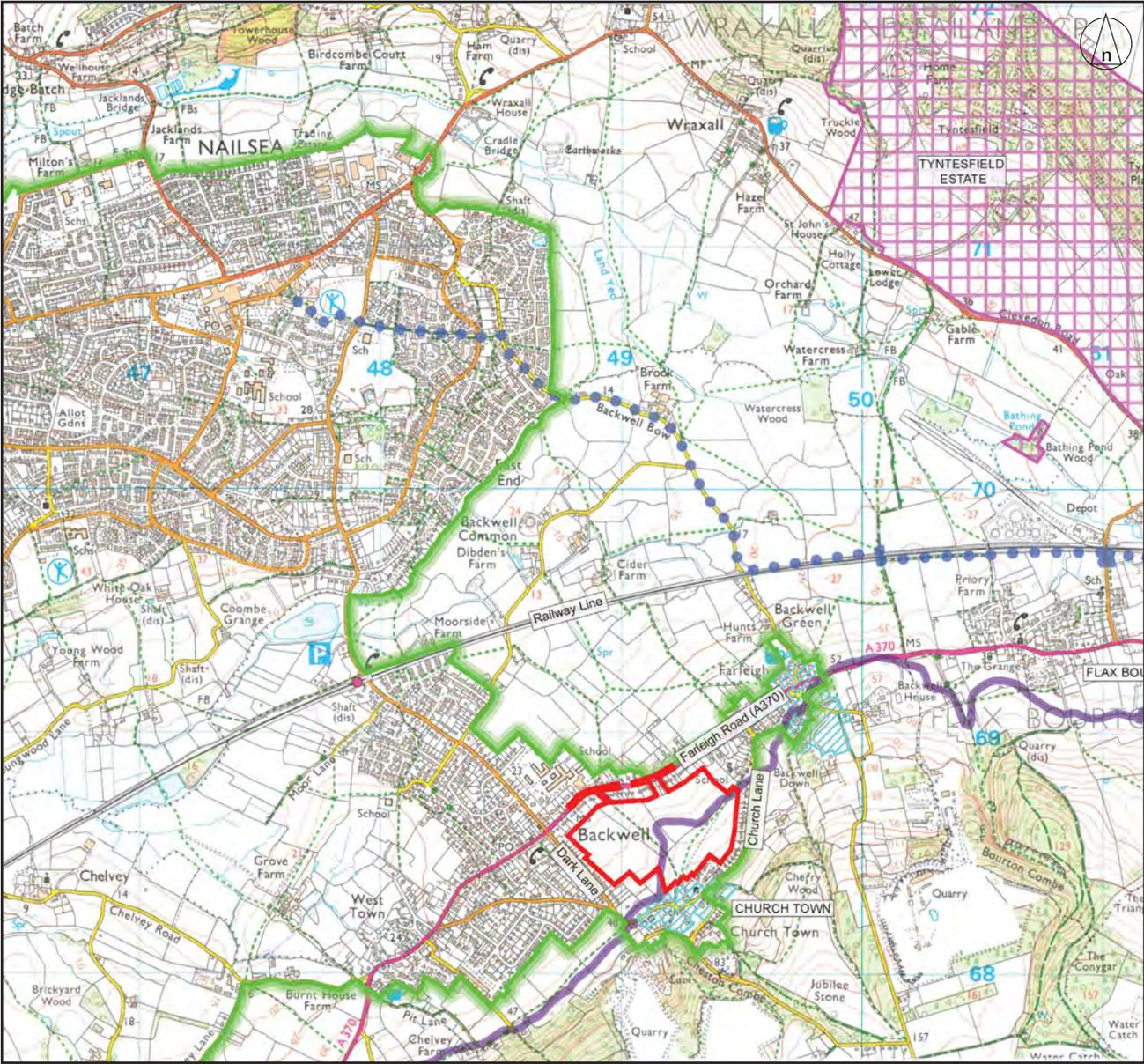
The lower, northern parts of the site has been assessed as being of 'low' landscape sensitivity, and the higher lying and more visible site area lying close to the Church Town Conservation area as being of 'medium to high' sensitivity.

**LEGEND**

-  Site boundary
-  Green Belt (Note 1)
-  Registered Park and Garden of Special Historic Interest (Note 2)
-  Conservation Area (Note 1)
-  Listed Buildings (Note 1)
-  Public Rights of Way (Note 3)
-  50m AOD contour: shown south of A370 (Note 3)
-  Festival Way (Note 4)

**Notes:**

- 1) Taken from North Somerset Local Plan, online Proposals Map, adopted March 2007. Listed Buildings only, shown to south of site.
- 2) Taken from Magic website, July 2014.
- 3) Taken from OS Explorer Map 154.
- 4) Taken from Sustrans website, August 2014.



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## 2.10 Drainage and Services

### Flood Risk

According to the Environment Agency's (EA) indicative flood map the site is located in a Flood Zone 1 area. These are areas which have a low probability risk of experiencing flooding from fluvial or coastal sources. 'Low Probability' areas are defined as having a less than 1 in 1000 annual probability of flooding.

As the site is over 1.0 hectare in size in accordance with National Policy and EA requirements, a Flood Risk Assessment (FRA) supports the application.

### Surface water drainage

A geotechnical survey of the site indicates that the soils under the site are impermeable, and therefore not suitable for the use of soakaways.

Farleigh Road along the frontage of the site is reported by the highway authority to receive overland surface water run-off from the adjoining frontage properties, including run-off from the site which flows through these properties.

Three surface water drainage strategy options are being investigated in liaison with the drainage authorities Wessex Water, NSC, IDB) to determine the optimum strategy. All options include provision of a storm-water attenuation pond located on the lower slopes of the site, and discharging flows at a restricted rate into a sewer which will convey flows towards Farleigh Road along the route of the existing agricultural access track.

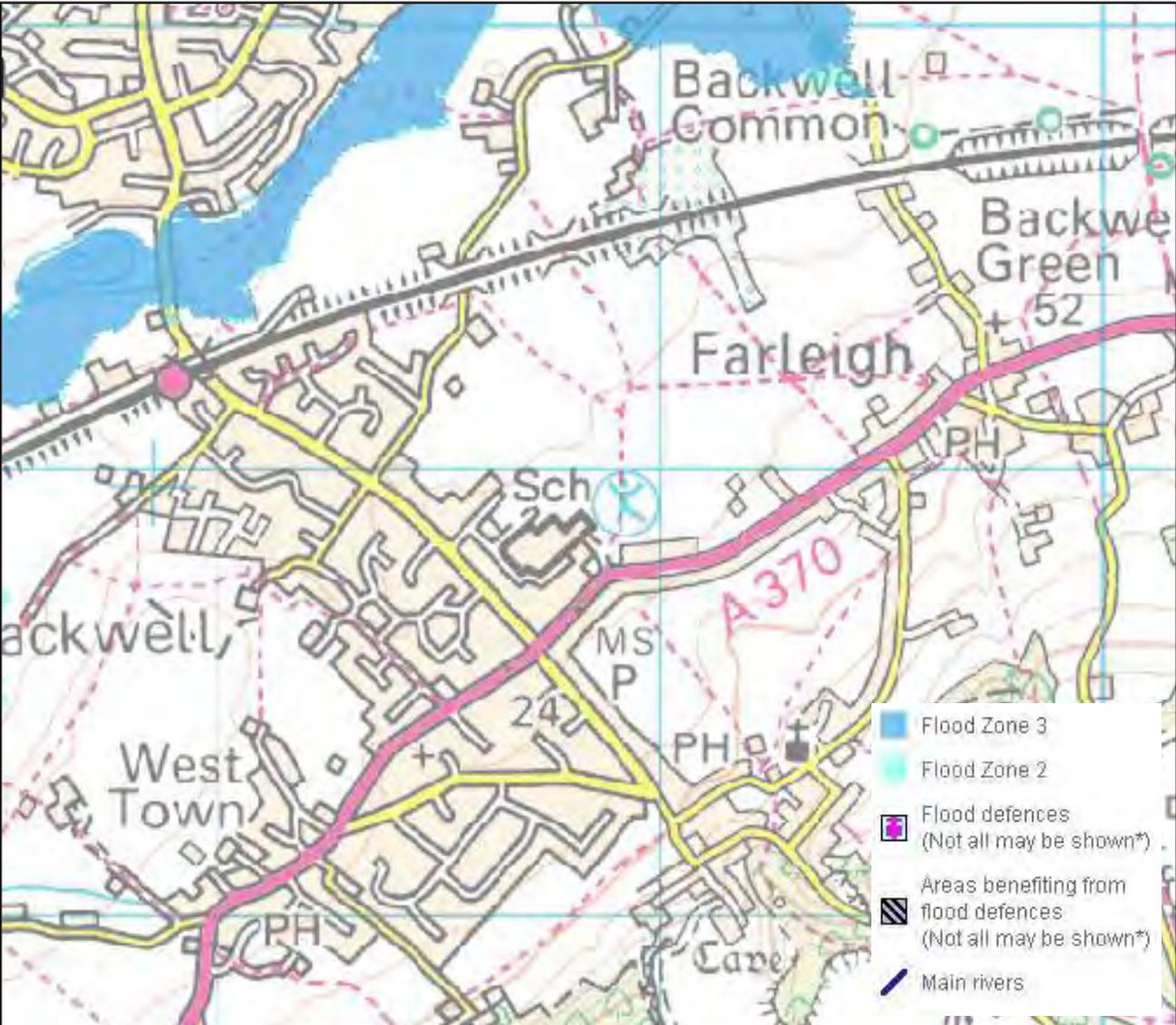
From Farleigh Road, the options being explored are:

- Discharge to an existing 375mm diameter adopted surface water sewer located at the junction of Lotts Avenue and West Town Road, approximately 300m west of the site.
- Discharge into the existing highway drainage running in Farleigh Road fronting the site, and promoting the downstream length of drain for adoption by Wessex Water as a public sewer.
- Discharge to an existing watercourse to the east of the NSC Leisure Centre, approximately 200m from the site.

### Foul water drainage

Foul drainage will discharge into an existing public foul sewer which runs along Farleigh Road fronting the site. Wessex Water Services are currently undertaking a scoping assessment to confirm if capacity is available to accommodate the site, and if not, what off-site reinforcement may be required.

Environment Agency Flood Map Extract



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Services

All the normal utilities are available in the local area.

BT and Virgin have services in Farleigh Road and can readily service the site.

Western Power Distribution can serve the site from its HV network in Farleigh Road – accepting that a substation will need to be located on the site.

Wales & West Utilities can serve gas to the site from their existing medium pressure main in Dark Lane.

## 2.11 Local Character

Good practice requires that a development such as this should in most circumstances demonstrate that it has taken into account a consideration of the influence of local character. A balance is to be struck between replicating local character (which may be good, bad or indifferent), providing a fresh injection of contemporary elements to mark the development as of its time, and ultimately synthesising these external influences into the new development.

There are two levels of character to be assessed. One level relates broadly to town planning, street patterns and dimensions, landscape and vegetation, the geometry of streets and routes, and the sense of enclosure or thoroughfare that they create. The other level relates to the architecture of individual buildings, how they relate to each other, and the extent to which they draw on local references such as roof pitches, materials, and fenestration. In between these two is the relationship between streets and buildings.

Backwell is typical of many villages that have grown steadily over the years. What is notable in Backwell is that growth has not always been in incremental developments of one or two dwellings but sometimes in surges at a point in time. The houses along Farleigh Road illustrate such a surge, apparently built within a small number of years, but on a plot-by-plot basis, with each house being different but nevertheless all observing an informal set-back building line which contributes to a sense of cohesive character.

It is for this reason that it is important to understand local character, and to identify the better traits that are to be drawn on to inform a new development.

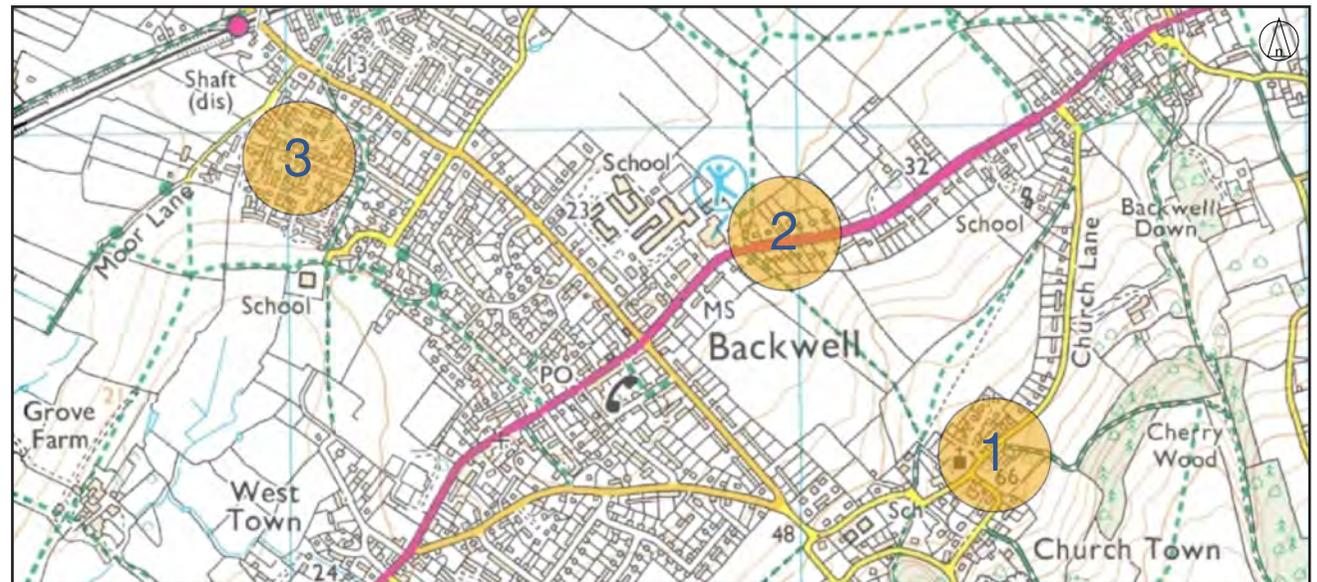
As part of the contextual analysis to inform the proposals, a sample of case studies has been undertaken of built form in the immediate vicinity.

These case study areas have been chosen due to their similar residential land use and proximity to the site to ensure that when used as a reference, the development can take elements from the local context. The positive aspects that inform local character together with more specific characteristics that differentiate them in terms of built form, architectural merit and function are summarised here.

The case study areas are:

1. Backwell Village
2. Farleigh Road
3. Long Thorn Development

### Local Character Plan



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### 1. Backwell Village

Backwell Church Town Conservation Area forms part of Backwell Village. Key characteristics of the area are:

- predominantly detached houses set in large plots;
- building lines are irregular with buildings often set far back from the road;
- on more historic buildings stone is the predominant building material;
- gables are common features on the more historic buildings;
- front gardens vary in size with a variety of boundary treatments, stone walls and hedges; and
- parking is to the front and side of dwellings.



A rural feel to roads



Historic properties within the conservation area



Large detached properties with front gardens

**Key influences to take forward**

- detached housing;
- a mix of house types;
- a considered variety of materials;
- on-plot parking; and
- gables with most roofs at 45 degrees.



Bungalows



On-plot parking to detached properties

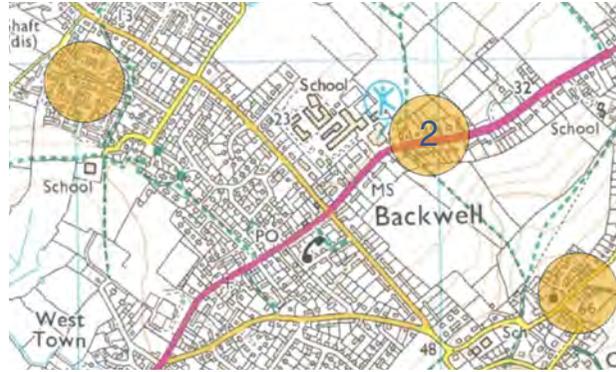
## 2. Farleigh Road

This area is adjacent to the site and therefore forms an important character reference. The character is inconsistent and irregular but can be described by;

- predominantly large detached properties in large plots;
- building lines generally following the street;
- a variety of red and buff brick and render;
- reconstituted stone cills;
- UPVC side hung casement windows;
- black rainwater goods;
- Minimum 4-5m front gardens with a variety of boundary treatments;
- long rear gardens; and
- parking to the front and side of dwellings.

### Key influences to take forward

- detached family housing;
- a variety of materials;
- on-plot parking; and
- gables.



Large front gardens



Mixture of boundary treatments



Large detached properties



Established tree planting along Farleigh Road



Farleigh Road

### 3. Long Thorn Development

The houses at Long Thorn are more representative of contemporary planning and house-building, rooted in a design approach established by CABI and other advisory bodies: a single developer has used a standard range of dwellings that are adapted to local circumstances and character, and at a density of about 34dph, including a mix of tenures and types. This recent development in Backwell forms perhaps a more relevant character reference, it is defined by;

- a 'Manual for Streets' layout;
- mainly 2 storey detached and semi-detached houses with short terraces of 2 and 2½ storey houses;
- modern and higher density of 38-40 dph;
- white PVC windows, black rainwater goods
- single storey render with re-constituted stone banding in some locations;
- square bays; and
- minimum 1.2m front buffer space with low shrub planting.



Detached properties with gables



Detached garages



Hedges as boundary treatments

**Key influences to take forward**

- density at 38-40 dph;
- detached family housing;
- terraced properties
- on-plot parking;
- gables; and
- adaptation of a tested range of dwellings to respond to local character.



Terraced properties

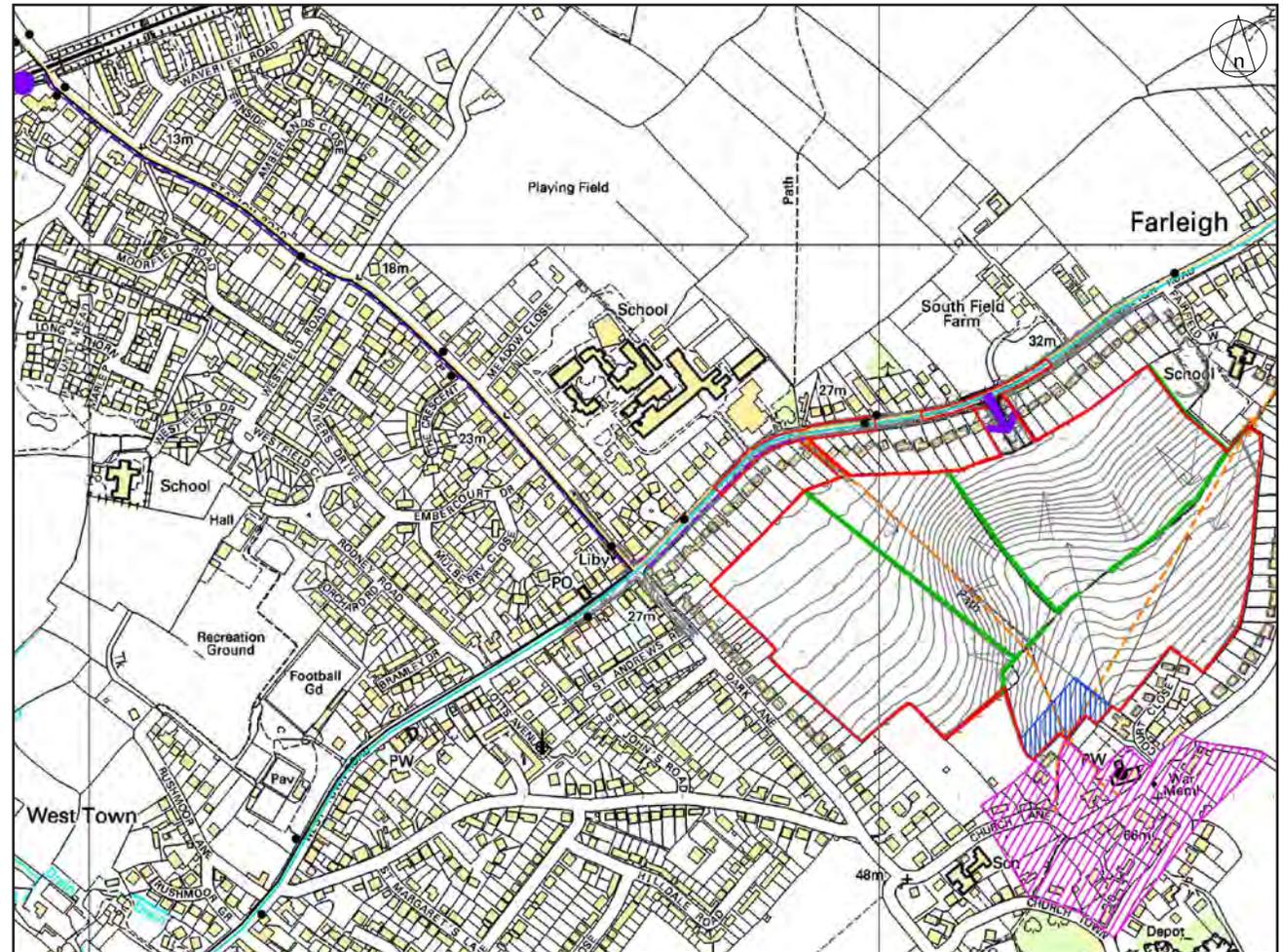


Tree planting around a focal space

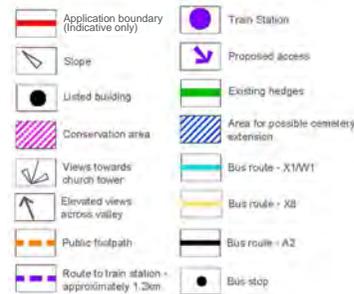
## 2.12 Constraints and Opportunities

The Technical Constraints and Opportunities Plan summarises the preceding assessment. The overarching proposal is to develop the lower, smaller portion of the site, and thereby to safeguard the remainder for community use as open space. The key opportunities and issues are:

1. Limits on Development: The inherent topography and the setting of the site and the nearby conservation area provide an opportunity to develop approximately the lower third of the site where the topography is conducive and where there will be little or no visual impact, especially considering views from and of the church and the conservation area.
2. Open Space: This leads to an opportunity to provide the opportunity to retain two thirds of the site for open space on the remaining two thirds of the site which can be transferred in perpetuity to local control or management, safeguarding it for future generations.
3. Future-proofing: The quantum of housing that can be delivered within this application could protect Backwell for many years from development pressures, and will deal with current demand in a sustainable and suitable location, with least impact.



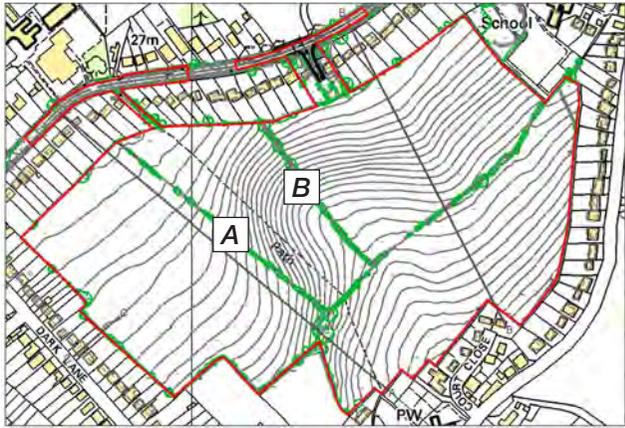
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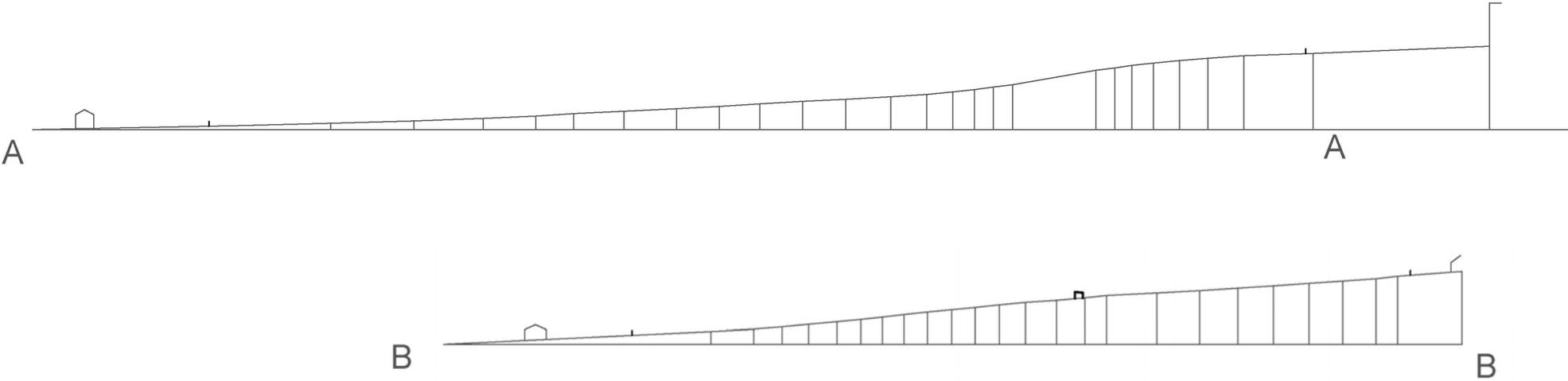
- 4. Housing Mix and Type: The quantum of housing will also allow a range of dwelling types and tenures that Backwell has acknowledged that it needs, including smaller homes for downsizers, and affordable homes, which would almost certainly not be delivered through incremental smaller-scale development.
- 5. Quality: Delivery will be by an established developer with a reputation for quality, ensuring high standards in the design of housing, the public realm and open spaces across the site.
- 6. Sustainability: This will be a highly sustainable development, making best use of land, and providing housing in a village that is, unusually, well served by a mainline station and full range of local facilities.

### Site Sections

The sections on this page demonstrate the topography of the site.



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# 3 INVOLVEMENT

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*“...a crucial first step to achieving good design is to develop a thorough understanding of the context within which the new housing will sit and then the nature of the site itself and its immediate surroundings”*

*(page 16 of Better Places to Live: By Design)*

## 3.1 Consultation / Local Engagement

In August 2014, representatives from Charles Church’s technical team met with planning officers from North Somerset District Council for a pre-application meeting to discuss the company’s interest in preparing and submitting a planning application to develop up to 340 homes on land at Farleigh Fields, Backwell.

This was followed by a meeting on Monday 29th September between representatives from Charles Church and two members of Backwell Parish Council, a representative from Backwell Neighbourhood Planning Group, Cllr Karen Barclay (NSDC Backwell ward member) and Cllr Geoff Coombs (NSDC Backwell Ward Member). The purpose of the meeting was to provide notice of Charles Church’s intention to submit an outline planning application and the core elements that would form the basis of the application. Consensus provided an explanation of how local residents would be informed about the company’s intentions and the recording and collation of public comments.

The Localism Act 2011 states that when implementing local consultation, the applicant must:

*“publicise the proposed application in such manner as the person (applicant) reasonably considers is likely to bring the proposed application to the attention of a majority of the persons who live at, or otherwise occupy, premises in the vicinity of the land.”*

The next step was to provide the community with an early insight into the development proposals and provide them with a means of recording their initial concerns, issues and aspirations.

Copies of INFORMATION, a tabloid-size newsletter, were sent to **1768** homes throughout Backwell. The newsletter outlined the development proposals and the policies and processes that would guide and inform the design and content of the application. A Feedback Form was also included which encouraged residents to write via Freepost or email to Consensus describing any concerns or issues they had about the development.

- Written responses via Freepost and email were received from **383** people at **290** properties .
- This is a response to invitation (to respond) rate of **16.4%**

Feedback.

The principal development issues and observations that were most frequently highlighted in the written feedback were:

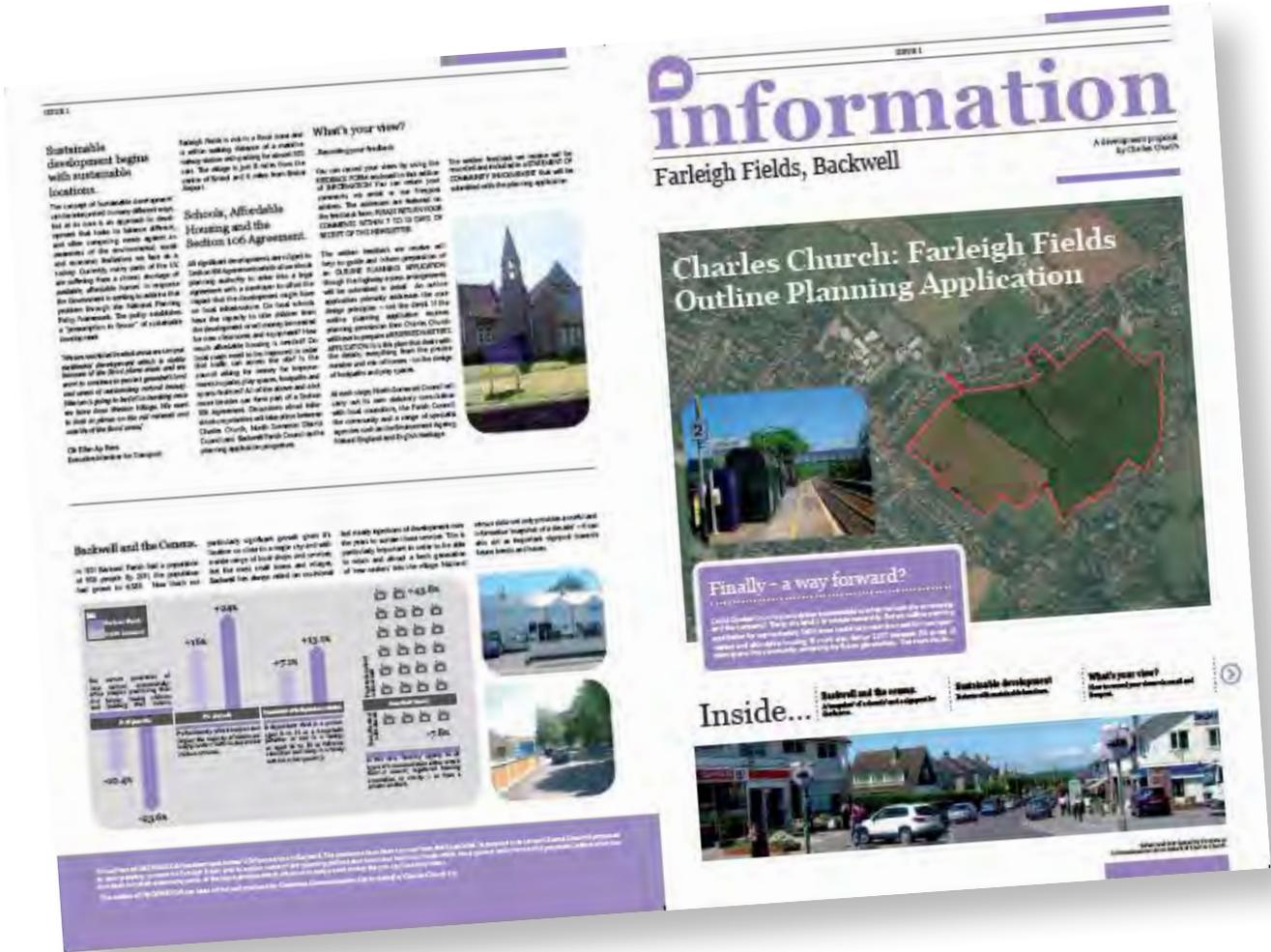
Traffic and Access 70.2%

Impact on local infrastructure 50.4%

Neighbourhood Plan (Proposed allocation of the site as local green space). 46.7%

Loss of a green field site. 43.3%

For a detailed summary of the applicants response to the issues raised please refer to the full SCI document which has been submitted in support of the application



# 4 EVALUATION

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## 4.1 Design Evolution

Design options have been explored for the site. These considered various layouts, responding to the constraints and opportunities for the site.

## 4.2 Evaluation

The assessment of the site and its context has provided the information to inform the development of the design on this site.

A summary of the outcomes of the assessment and involvement is provided below. This evaluation has in turn informed the design principles in the following chapters.

### Amount

The amount of development that is considered appropriate for the site has been assessed. Locating the development on the northern, lower part of the site minimises any visual impact. 12.88ha of the site has been kept free of development, providing a generous amount of open space that will be permanently available to the public.

### Layout

The proposed layout and the structure of the masterplan has been informed by the context and by the constraints and opportunities of the site. The development has addressed concerns raised as a result of the consultation, as well as responding to the features of the site such as the topography.

### Scale

The scale of the proposed development has been informed by the assessment of the site and the local context. The surrounding residential areas are largely two-storey. The proposed building heights reflect this and the need to assist legibility and the creation of a sense of place. Buildings will be predominantly 2 storey with some of 2.5 storeys in key locations and on corners.

### Landscaping

The existing landscape features of the site, including key views have been assessed to inform the masterplan. The proposed development retains existing trees and hedges and creates large areas of open space. The topography and visual assessment has influenced the masterplan.

## Appearance

The local character assessment identified a number of distinctive qualities of the existing local area and potential design influences.

This information has influenced the principles for the proposed appearance and layout. The development principles provide for a development of starter homes through to family housing set within a strong landscape setting, that provides a generous amount of open space and takes its cues from its surroundings and the more traditional housing in the local area.

This will become more apparent and will be presented in more detail in subsequent reserved matters applications.



# 5 DESIGN PARAMETERS

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## 5.1 Concept Plan

The site has been assessed and the concept plan opposite is a result of site assessment and involvement which is outlined in the previous sections.

This plan provides the framework for the design parameters set out in the following sections.

These parameters set the extent of the development that is to be approved in this outline planning application.

The Concept Plan shows the following key elements;

- allowances have been made for highways, drainage and the protection of the amenity and privacy of surrounding perimeter properties;
- open space is provided on the site in excess of standards set out by the local authority. 12.88ha of open space could be made permanently available, made up of 1.73ha required open space associated with development and 10.32ha as additional open space;
- potential additional pedestrian and cycle access points are located around the edges of the site at key locations;
- the site entrance will provide a space that creates a sense of arrival and helps to calm traffic speeds;
- there will be clearly defined pedestrian and cycle routes leading from the entrance space to ensure good connectivity to the rest of the scheme and into Backwell;
- connections to the local area;
- an average density of about 25dph is considered appropriate in this location for the developed part of the site with the majority of the remaining site retained as open space; and
- there is an opportunity to provide a generous proportion of open space and to provide facilities such as play equipment.

### Concept Plan



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## 5.2 Land Use

The proposed development includes residential development and includes 7.29ha of developable land.

### Residential

Details of the development will be provided at reserved matters stage. The likely amount of development may be up to 220 units and could consist of predominantly detached and semi-detached housing with some terraces.

### Land Budget Parameter Plan



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### 5.3 Scale

The houses proposed are a mixture of detached and semi-detached dwellings with some terraces. They will be mostly 2 storey with scope for up to 2.5 storey buildings in key locations.

There will be only 2 storey houses on most of the plots that back on to existing gardens, with 2.5 storey houses deployed to emphasise the site entrance, focal point, and corners.

Building Heights Parameter Plan



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## 5.4 Density

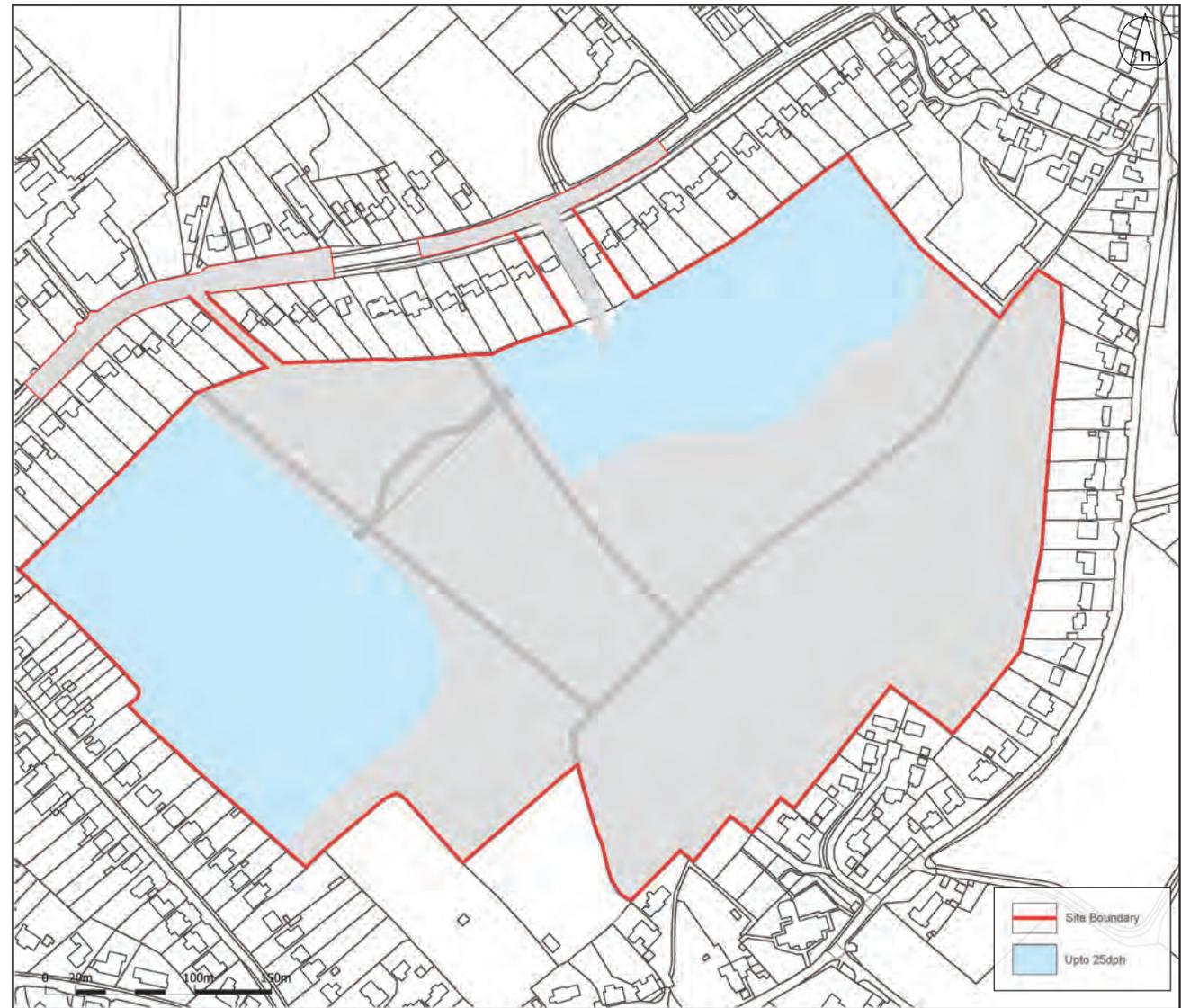
The net density of the proposed development is 25 dwellings per hectare (dph). The net density of the most comparable development in Backwell, at Long Thorn, is about 34 dph.

North Somerset's remitted proposed Core Strategy policy CS14 requires a "... target net density across North Somerset... 40 dwellings per hectare, although this may be higher at highly accessible locations...". Our interpretation of Policy CS14 would interpret Backwell as a 'highly sustainable location', the proposal for 25 dph would therefore be reflective of that policy.

In the absence of any guidance within the NPPF/ NPPG in relation to density calculation we have used the standard measure as set down in the now replaced PPG3 (in turn drawn from 'The Use of Density in Urban Planning') which includes incidental open space and landscaping, but excludes major distributor roads, schools, open space serving a wider area, and significant landscape buffer strips.

For the avoidance of doubt, the calculation of net density here takes into account the two development parcels totalling 7.29ha, and the 1.73ha of required open space between them, so that up to 220 dwellings on 9.02ha yields a net density of 25 dph.

### Density Parameter Plan



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### 5.5 Access and Movement

The parameter plan opposite defines the vehicular and pedestrian access points. It also outlines the primary movement routes within the site and the network of pedestrian and cycle routes and public rights of way.

#### Pedestrian/Cycle Access

The parameter plan provides for efficient movement by pedestrians and cyclists following key desire lines across the site. It also identifies existing public rights of way and other existing pedestrian accesses into the site.

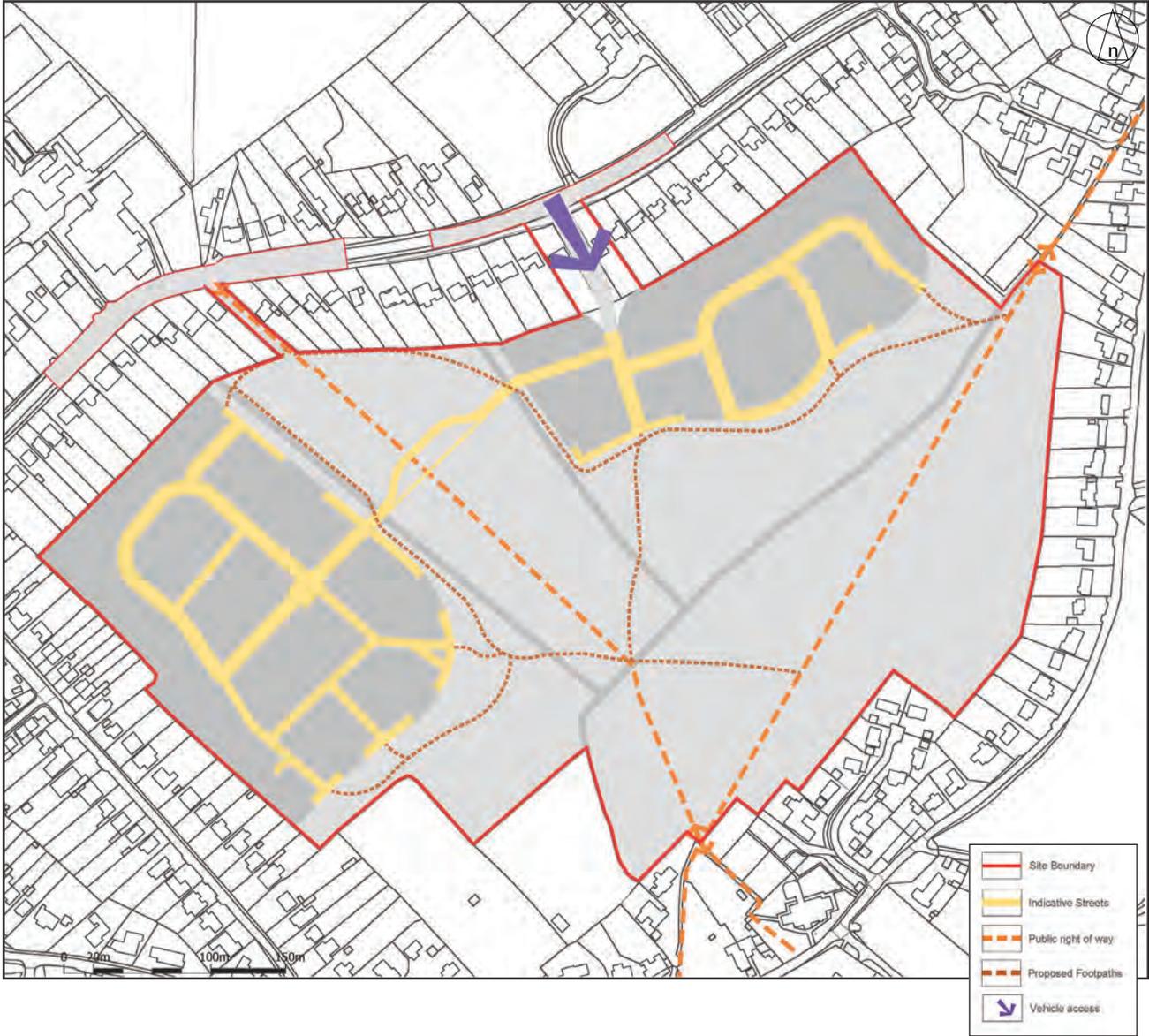
The masterplan illustrates the proposed road layout which will be carried forward as part of any subsequent Reserved Matters application. The plan provides quiet streets which will form part of the movement framework for pedestrians and cyclists. It will include facilities for disabled people such as dropped kerb and tactile paving as appropriate.

The Transport Assessment has identified opportunities to improve existing off-site infrastructure and includes proposals to install pedestrian crossings across Farleigh Road on the route to Backwell School and local bus stops.

#### Vehicular Access

The site will be accessed from Farleigh Road via a T-junction with a ghost island arrangement.

Access and Movement Parameter Plan



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## 5.6 Vehicular Access Points

The site will be accessed from Farleigh Road between 54 and 58 Farleigh Road via a T-junction with a ghost island arrangement by way of a priority junction with a ghost island right turn lane.

Farleigh Road is a high quality road with footways on both sides and there is enough space around the proposed access point to allow for sufficient visibility splays. The proposed site access arrangements will be safe and suitable for all people in accordance with the NPPF.

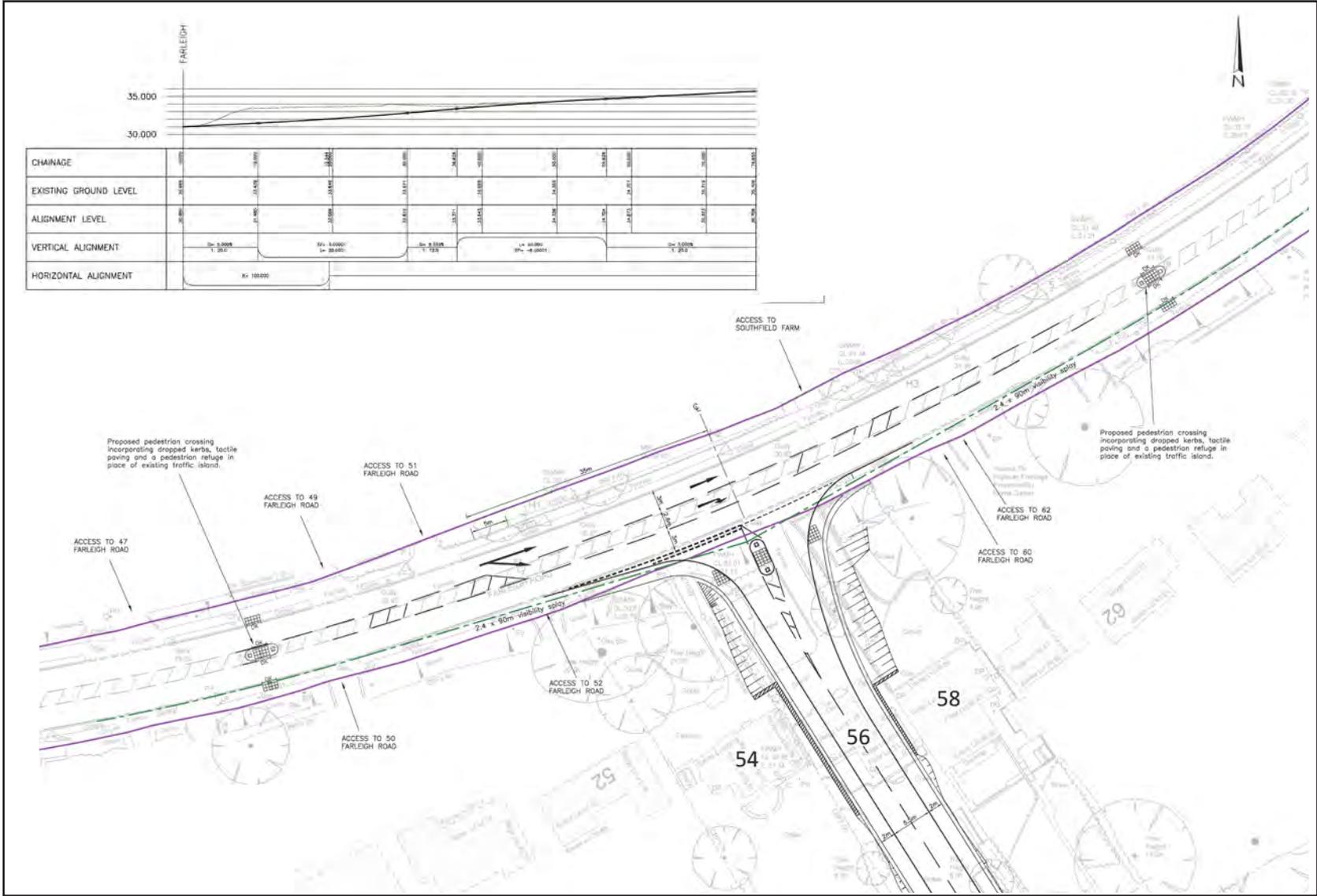


*Proposed point of access onto Farleigh Road*



*Farleigh Road*

### Proposed Access Plan



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# 6 DESIGN STRATEGIES AND PRINCIPLES

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This chapter presents design proposals which have been developed and informed through assessment, involvement and evaluation. This illustrative information explains how the principles established by the design parameters in the previous chapter might be applied when the proposals are developed in the future.

The design and access statement should provide principles for the strategic arrangement of built form and spaces as well as explaining the principles behind the intended appearance and how these will inform the final design.

## Illustrative Masterplan

The Illustrative Masterplan has been revised in direct response to comments received from officers and the local community. The revised plan demonstrates how the principles outlined in the previous sections could be applied when designing a layout for the site.

The proposed development will form a sustainable expansion of the village. It will respect its surroundings and create new connections and will provide much-needed housing without having to extend the outer edges of the village into the countryside. The site helps to promote sustainable forms of transport and its location within walking distance of Nailsea and Backwell mainline train station and bus stops helps reduce car dependency which is high in Backwell. There are direct and easy connections to local facilities and to the wider countryside.

The key objective is to create an attractive new neighbourhood that respects important landscape setting and reflects the character of Backwell.

## Appearance

The local character study in Section 2 identified a number of features that are local to the area in terms of built form and layout. Those features of merit outlined

in Section 2 will where appropriate be reflected in subsequent detailed design proposals.

The illustrative design of the houses shown in this section take their cue from the steep pitched roofs and the mixture of bays, gables and dormers, small porches and brick detailing that are part of the character of Backwell. These features would be applied to elevations in a layout that reflects the particular characteristics of the site.

## Car Parking

Parking provision (including motor vehicles, motor cycles and cycle parking) will meet the standards set by the Council. A combination of well-designed on-street and on-plot parking, together with some grouped parking will be used according to the character area, location and market conditions. All allocated parking spaces will be located as close as is practicable to the dwellings. Grouped parking and courtyards, if any, will be designed as part of the shared space of the block and parking limited to 6 dwellings. Visitor parking will be provided on-street or within the shared parking courtyards.

### Illustrative Masterplan



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## 6.1 Character Areas and Appearance

The assessment of the site and the surrounding area has informed the designation of three character areas. The aim of these character areas is to create a neighbourhood with a distinct sense of place that responds to the features of the site.

This section provides the key design themes covering the following three character areas:

### Entrance

The gateway to the site, buildings will be arranged to create a sense of arrival

### Residential Core

This area that makes up the majority of the site.

### Development Edge

This more relaxed edge responds to the open landscape setting of the site and the existing properties.

### Central Space

This space is the focal space for the neighbourhood.

Character Areas Plan



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## Entrance

Buildings here will help to create a **sense of arrival** through their **scale and positioning**.

The built form may include some buildings of up to 2.5 storeys in key locations. Houses will be mainly semi-detached or terraced with front gardens and clearly defined boundaries. There will also be some dwellings with integral garages. The exact details will be determined at reserved matters stage.



## Residential Core

This area makes up the **core of the development** and has a character resonant with that of Backwell.

The built form will be **predominately 2 storeys** with up to 2.5 storeys in key locations. Trees will be at strategic locations along the main street and will break up larger parking areas.

Parking will be mainly on-street or on-plot at right angles to the street behind the building line.



### Development Edge

This area will be visually more prominent towards the southern edge of the site fronting the open space.

Properties will be predominately detached and semi-detached units. Units will be **arranged more formally**. Building heights will be **mostly 2 storey** with limited use of 2.5 storey houses on corners and in key locations.

Tree planting will be within the open space.

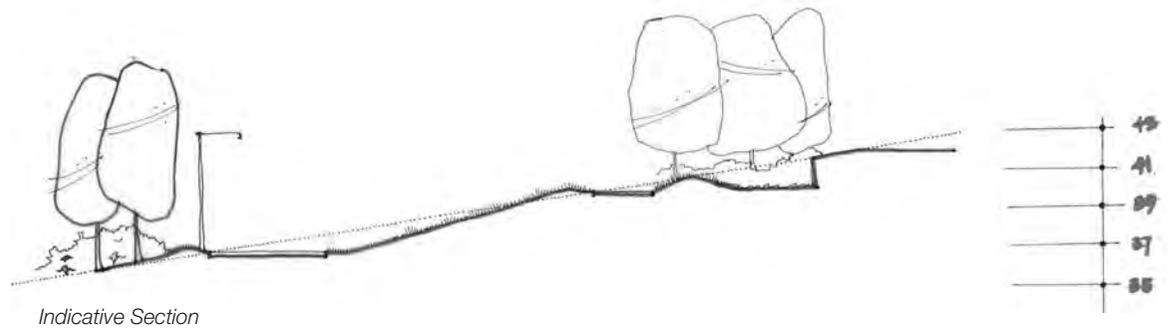
Parking will be mainly on plot to the side of dwellings with garages set back behind the building line.



## Central Space

The space here creates a focal space for the neighbourhood, including play facilities and informal open space. The carriageway of the road is split to reduce visual impact.

The view to St Andrews Church is retained and there is an integrated network of footpaths.



## 6.2 Landscape and Open Spaces

The landscape strategy aims to create an attractive setting for the development to help it to be assimilated into the surroundings and enhance the existing landscape framework.

Key principles of the landscape strategy are listed below:

- provide a strong, green framework, designed to integrate the proposals into their surroundings;
- protect and enhance the existing vegetation framework through new complimentary planting and management;
- retain a buffer of undeveloped land between the new housing and Church Town Conservation Area;
- retain key views towards the listed church in Church

Town, and also, views across the valley from opposite the church;

- retain the existing public footpaths with new ‘gateway’ tree planting at entrance points to create a ‘sense of arrival’, and provide new linkages across the site;
- create a multi-functional open space incorporating a range of informal recreational uses, attenuation basins and enhanced wildlife habitat areas.



## 6.3 Street Hierarchy

The plan following shows how the different street types are arranged in the illustrative layout. The main features of the streets are described in the following paragraphs and are used to reinforce the hierarchy of each route:

### Street

This street provides the main access into and around the site. It allows access to the other routes. Buildings will form a largely continuous building line and will be mainly 2 storeys with up to 2.5 storeys in key locations.

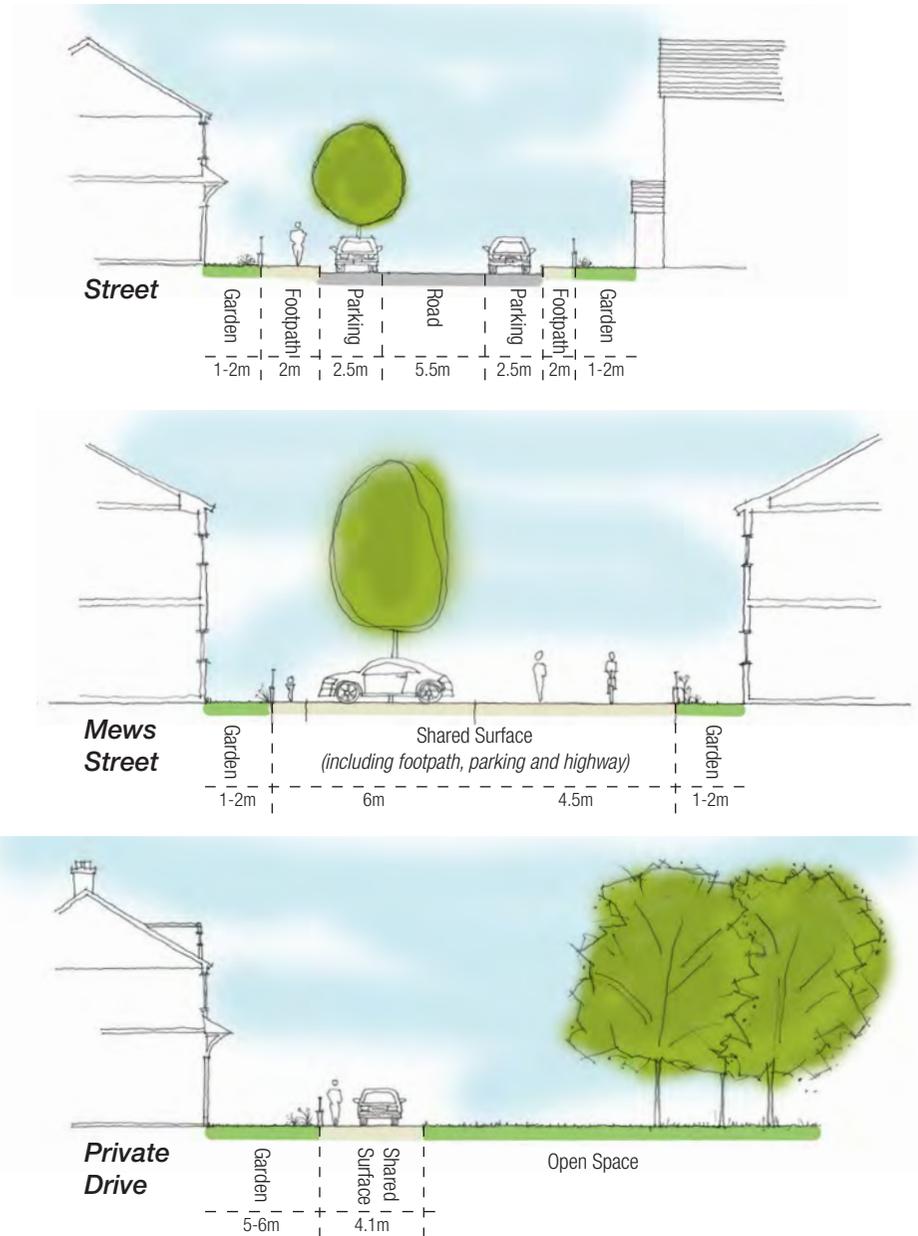
### Mews Streets

These smaller streets provide links through the site. Traffic levels will be low and Manual for Streets principles will allow pedestrians and cyclists to move safely through. Built form will be mainly terraced and semi-detached and 2 storeys with 2.5 storeys in key locations.

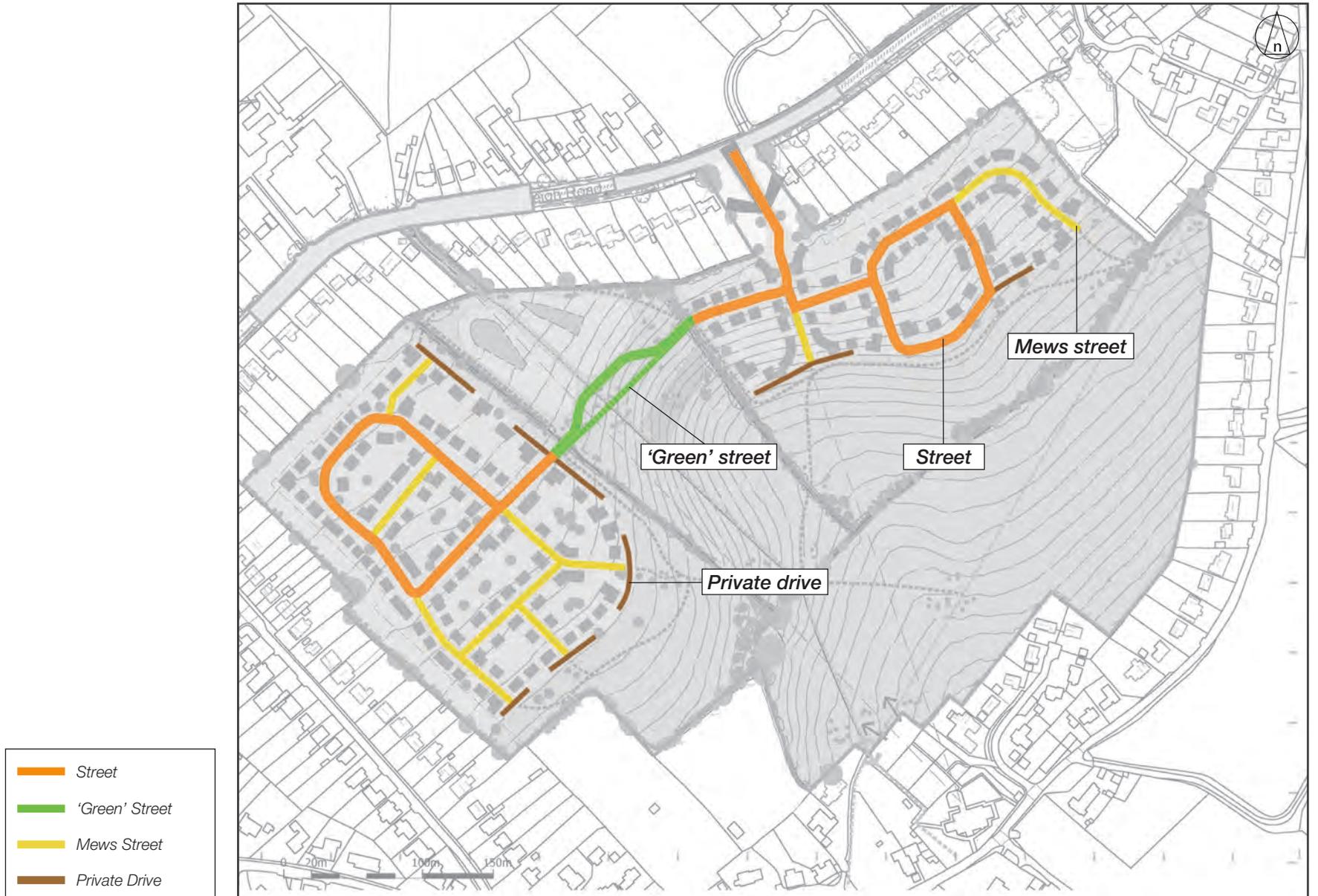
### Shared Surface/Private Drive

These shared surface routes include private drives and courts and only serve a limited number of properties. The low levels of traffic allow shared surfaces to safely accommodate pedestrians, cyclists and vehicles within one space. They may provide links to the surrounding footpath network and open space beyond.

Tree positions are illustrative and will be clarified in a reserved matters submission. Depending on site conditions they may be in private gardens, open space, or in streets.



### Street Hierarchy Plan



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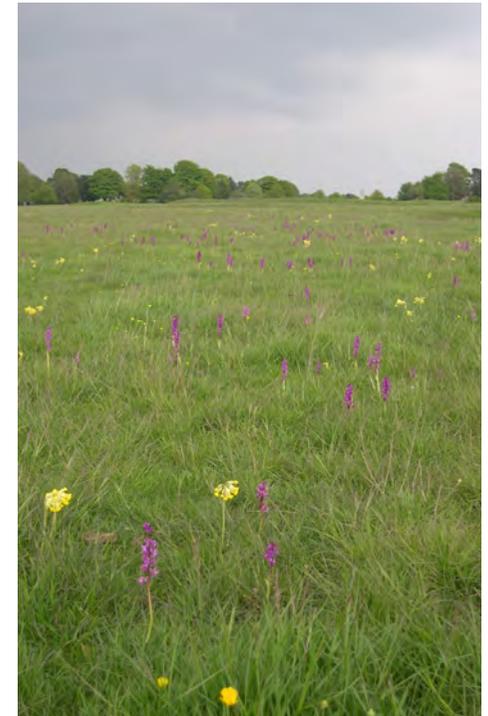
## 6.4 Sustainability

The Energy Statement has demonstrated how the proposed Farleigh Fields development will be designed using the principles of the Energy Hierarchy in order to deliver a 42% reduction in energy demand when compared to a Part L 2010 compliant building, in order to achieve compliance with Policies CS1 and CS2 of the North Somerset Core Strategy. The design team have opted for a strategy utilising the principles of passive design and energy efficiency. This approach is advantageous in ensuring reduction in energy

consumption at source, rather than installation of costly renewable measures that only provide a means of offsetting energy consumption, rather than controlling demand.

A detailed review of the possible heating and energy strategies for the development has been carried out, and concluded that the use of high efficiency gas boilers is considered the most appropriate for the proposed dwellings. The practicality of all primary LZC

technologies for the site has been assessed. There are limited practically or commercially viable opportunities for micro-CHP, biomass, wind and decentralised CHP at the site. Although photovoltaics, solar thermal and air/ground source heat pumps are considered technically feasible, integration of such technologies would provide additional capital expenditure requirements of between c. £1,088,000 and £3,060,000. Subsequently, these technologies have been discounted by the applicant on the grounds of commercial viability.



### 6.5 Phasing

Following the grant of outline planning permission, it is anticipated that reserved matters proposals would be worked up for the site along with discharging any appropriate pre-commencement conditions. Ground works and infrastructure delivery would then occur.

As part of the development process a phasing plan would be agreed with the LPA. This would reflect the concept masterplan and associated parameter plans. It is anticipated that the phasing (two stages) would ensure the residential development occurred in a coordinated manner.

The plan shown here is illustrative of a likely approach to phasing.

Phasing Plan



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## 6.6 Summary and Conclusion

This design and access statement has set out the principles and parameters for the design and access of the proposed development, in support of an outline planning application.

The vision for this development is to provide a sustainable development that respects the site's existing features and creates an attractive and well integrated neighbourhood in Backwell. The application addresses this as follows:

### Amount and Mix

- developable area of 7.29ha;
- a mix of detached, semi-detached and terraced units;
- required open space 1.73ha; and
- additional open space 10.32ha.



View from site towards St Andrews Church

### Scale and Massing

- mainly 2 storeys; and
- 2.5 storeys in key locations.

### Layout and Accessibility

- three key street types: main street, mews street and private drives:
- the main street forms the main access;
- the mews streets create the network serving the development;
- the private drives serve only a few properties; and
- streets will be designed to Manual for Streets principles where appropriate.



Backwell Local Centre

### Character Areas

- there are three character areas which reflect the different characteristics of the site.

### Landscape and Open Spaces

- opportunities for informal recreation;
- retention of footpath link connecting through the development to Farleigh Road; and
- provision of additional footpath links and formal play facilities.

### Materials

- likely building materials are red brick and render, with a mixture of gray and clay roofing tiles; and
- other preferred features are open porches and low front boundary walls.



Nailsea and Backwell Train Station



# FARLEIGH FIELDS, BACKWELL